

4/10/2024 3:54:15 PM
\\TDO\T03\NAS002\tdot.state.tn.us\03\Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Maury Co. SR-6 L.M. 9.30 - 14.8\1129517-00 000 Signature Sheet.sht



4-10-24

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Nicholas Kniazewycz
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TENNESSEE DEPARTMENT OF TRANSPORTATION
6601 CENTENNIAL BLVD.,
NASHVILLE, TN 37243
NICK KNIAZEWCZ, P.E. NO. 126392

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2A
GENERAL NOTES	2B
SPECIAL NOTES	2C
ENVIRONMENTAL NOTES	2D
TABULATED QUANTITIES	2E
RIGHT-OF-WAY NOTES, UTILITY NOTES, AND UTILITY OWNERS	3
PROPERTY MAP AND RIGHT-OF-WAY ACQUISITION TABLE	3A
PRESENT LAYOUT	4
RIGHT-OF-WAY DETAILS	4A

YEAR	PROJECT NO.	SHEET NO.
2024	STP-NH/HSIP-6(150)	ROADWAY-SIGN1

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
SIGNATURE SHEET



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:
Matthew McKay
Digitally signed by Matthew McKay
Date: 2025.04.16 08:16:32 -05'00'

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TENNESSEE DEPARTMENT OF TRANSPORTATION
6601 CENTENNIAL BLVD.,
NASHVILLE, TN 37243
MATTHEW MCKAY, P.E. NO. 120464

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET.....	ROADWAY-SIGN2
TITLE SHEET.....	1
ROADWAY INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE.....	2A
GENERAL NOTES	2B
SPECIAL NOTES	2C
ENVIRONMENTAL NOTES	2D
TABULATED QUANTITIES.....	2E
RIGHT-OF-WAY NOTES, UTILITY NOTES, AND UTILITY OWNERS	3
PROPERTY MAP AND RIGHT-OF-WAY ACQUISITION TABLE	3A
PRESENT LAYOUT	4
RIGHT-OF-WAY DETAILS	4A

YEAR	PROJECT NO.	SHEET NO.
2025	STP-NH/HSIP-6(150)	ROADWAY-SIGN2

REV. 4-9-25: ADDED SHEET TO PLANS.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

16-APR-2025 07:44
\\TDOT03NAS002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Maury Co. SR-6 L.M. 9.30 - 14.81\129517-00 001 Title Sheet.dgn

INDEX OF SHEETS
SEE SHEET 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

MAURY COUNTY

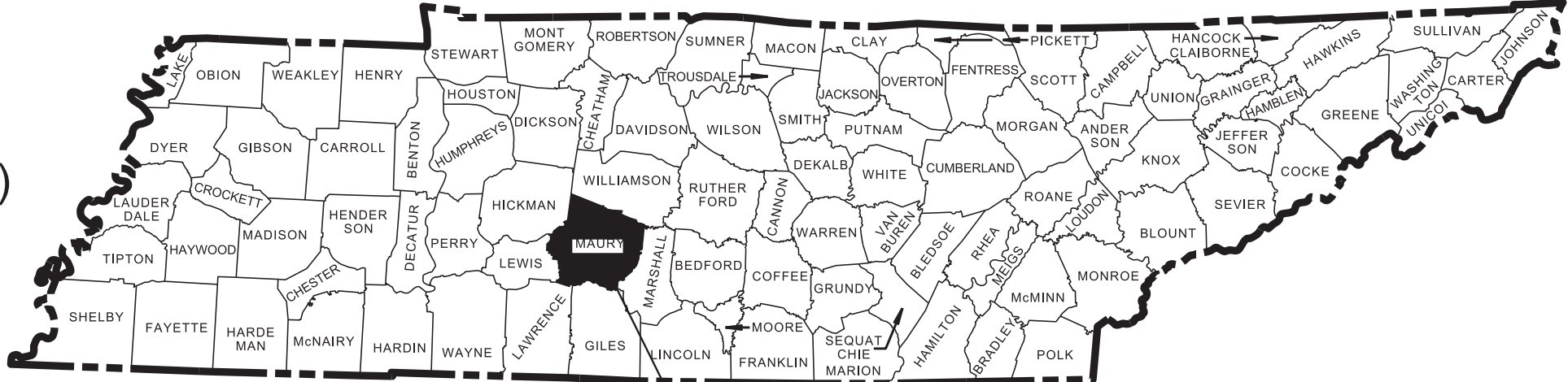
STATE ROUTE 6
FROM NEAR GREENWOOD STREET/SOUTH CROSS BRIDGES ROAD (L.M. 9.30)
TO BRIDGE OVER TS RAILROAD (L.M. 14.81)

RESURFACE & SAFETY
MILL & 411D

STATE HIGHWAY NO. 6 F.A.H.S. NO. 43

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO _

TENN.	YEAR 2025	SHEET NO. 1
FED. AID PROJ. NO.	STP-NH/HSIP-6(150)	
STATE PROJ. NO.	60S006-M3-006	
STATE PROJ. NO.	60103-3212-94	
STATE PROJ. NO.	60103-8212-14	



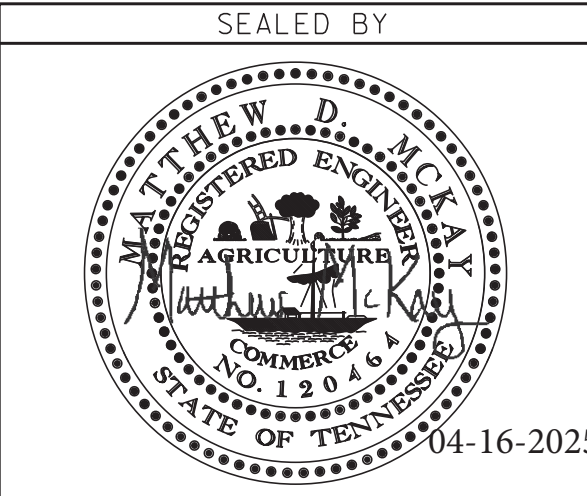
PROJECT LOCATION


BRIDGE ID. # 60SR0060033 60SR0060034 60SR0060035
60SR0060071 60SR0060077 60SR0060078
60SR0060037 60SR0060038

NO EXCLUSIONS

PROJECT TO BE LET
WITH PIN 129518.00

REV. 4-9-25: UPDATED PROJECT
YEAR TO 2025 ON ALL SHEETS.
ADDED PROJECT BUNDLE NOTE.



APPROVED: 
WILL REID, DEPUTY COMMISSIONER /
CHIEF ENGINEER

DATE: _____

APPROVED: 
HOWARD H. ELEY, DEPUTY GOVERNOR &
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

#60SR0060037
TSRR [938826H] OVERPASS CROSSING #60SR0060038

TSRR M.P. 0237.53
L.M. 14.81

60S006-M3-006
60103-3212-94
60103-8212-14

END PROJECT NO. STP-NH/HSIP-6(150) RESURFACE & SAFETY
L.M. 14.81

60S006-M3-006
60103-3212-94
60103-8212-14
BEGIN PROJECT NO. STP-NH/HSIP-6(150) RESURFACE & SAFETY
L.M. 9.30

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS
AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER: KONNER SPRADLIN, P.E.
DESIGNER : SAMUEL ANDERSON CHECKED BY : CAYLIE MARVEL
P.E. NO. 98034-4299-04 (DESIGN)
PIN NO. 129517.00

PROJECT LENGTH 5.51 MILES
TOTAL LANE MILES RESURFACED 22.04 MILES



TRAFFIC DATA

ADT (2022)	18,113
L.M.	POSTED SPEED
9.30 - 11.42	55 MPH
11.42 - 14.81	65 MPH

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ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1,2
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GENERAL NOTES	2B
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PAVEMENT MARKING IMPROVEMENTS.....	2F-2F7
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BRIDGE PLANS	B1

NOTE: NO UTILITY SHEETS.

THE ALPHABETICAL LETTERS “I”, “O”, & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATION AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
SAFETY DESIGN AND GUARDRAILS		
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINALS (RETROFIT)
S-GRT-3	06-28-19	TYPE 21 GUARDRAIL END TERMINAL


STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
SIGNS		
T-S-9	06-10-14	STANDARD LAYOUT GROUND MOUNTED SIGNS
T-S-10	04-04-12	STANDARD MOUNTING DETAILS FLAT SHEET SIGNS ALUMINUM-STEEL DESIGN
T-S-16	07-02-15	GROUND MOUNTED ROADSIDE SIGN PLACEMENT DETAILS
T-S-17	07-11-17	STANDARD GROUND MOUNTED SIGN USING PERFORATED/ KNOCKOUT SQUARE TUBE
T-S-20	07-11-17	SIGN DETAILS
PAVEMENT MARKINGS		
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-M-6	01-24-25	MARKING DETAIL FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-9	01-24-25	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9B	01-24-25	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-15	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATES AND ACCESS-CONTROLLED ROUTES
T-M-15A	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS-CONTROLLED RURAL ROUTES
T-M-18	01-24-25	FLEXIBLE DELINEATOR DETAILS
T-M-18A	01-24-25	DELINEATOR MOUNTING DETAILS
T-M-19	01-24-25	PAVEMENT MARKING DETAILS FOR RESTRICTED CROSSING & J-TURN INTERSECTIONS
WORK ZONES		
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63	01-09-24	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	1A

REV. 4-9-25: UPDATED REV. DATES AND TITLES TO VARIOUS STD. DWGS. ADDED ROADWAY-SIGN2 TO PROJECT INDEX.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION


ROADWAY INDEX
AND STANDARD
DRAWINGS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON THE FOLLOWING BRIDGES AND NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	
EDHZ002	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	SEE NOTE EDHZ001.	BRIDGE NO. 60SR0060077 SR-6 NB OVER ZION ROAD LM 13.61 (60-SR006-13.61R)
			BRIDGE NO. 60SR0060078 SR-6 SB OVER ZION ROAD LM 13.61 (60-SR006-13.61R)
EDHZ003	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	SEE NOTE EDHZ001.	BRIDGE NO. 60SR0060037 SR-6 NB OVER TN SOUTHERN RR LM 14.71 (60-SR006-14.71R)
			BRIDGE NO. 60SR0060038 SR-6 SB OVER TN SOUTHERN RR LM 14.71 (60-SR006-14.71L)

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PROJECT
COMMITMENTS

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\\DOT03NAS002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Maury Co. SR-6 L.M. 9.30 - 14.81\129517-00 002 Estimated Roadway Quantities.dgn

ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 60103-8212-14	QUANTITY 60103-3212-94	TOTAL QUANTITY
(1)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	22.1		22.1
(2)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	1094		1094
(3)	307-02.08 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING B-M2	TON	164		164
(4)	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	131		131
(5)	411-02.10 ACS MIX(PG70-22) GRADING D	TON	18659		18659
	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	9		9
	411-12.02 SCORING SHOULDERS (NON-CONTINUOUS) (16IN WIDTH)	L.M.	7		7
(6)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	18465		18465
	705-02.10 GUARDRAIL TRANSITION 27IN TO 31IN	EACH		4	4
(7)	705-04.09 EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH		2	2
(7)	705-04.10 EARTH PAD FOR GUARD RAIL END TREATMENT	EACH		2	2
	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		2	2
	705-06.30 GR TERMINAL (ENERGY ABSORBING) MASH TL2	EACH		2	2
	706-01 GUARDRAIL REMOVED	L.F.	300		300
(8)	712-01 TRAFFIC CONTROL	LS	0.9		0.9
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	200		200
(9)(10)	712-06 SIGNS (CONSTRUCTION)	S.F.	2052		2052
	712-08.03 ARROW BOARD (TYPE C)	EACH	2		2
	712-08.12 QUEUE PROTECTION TRUCK	DAY	240		240
(11)	713-02.14 FLEXIBLE DELINEATOR (WHITE)	EACH	19		19
(11)	713-02.15 FLEXIBLE DELINEATOR (YELLOW)	EACH	3		3
(12)	713-15.06 MILE POST SIGN	EACH	10		10
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	2		2
(13)	713-16.20 SIGNS (R1-2, YIELD)	EACH		4	4
(14)	713-16.50 REMOVE AND REPLACE SIGN (W4-1R, MERGE SYMBOL)	EACH	1		1
(15)	713-16.51 REMOVE AND REPLACE SIGN (I-5, AIRPORT SYMBOL)	EACH	1		1
(15)	713-16.52 REMOVE AND REPLACE SIGN (M6-2R, ARROW)	EACH	1		1
(16)	713-16.53 REMOVE AND REPLACE SIGN (D3-2, DESTINATION WITH LT ARROW, "ZION RD")	EACH	1		1
(17)	713-16.55 REMOVE AND REPLACE SIGN (M1-4B, U.S. ROUTE MARKER, "43")	EACH	1		1
(18)	713-16.56 REMOVE AND REPLACE SIGN (M6-1R, RIGHT ARROW)	EACH	1		1
	716-01.22 SNOWPLOWABLE RAISED PAVEMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	728		728
	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	280		280
	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	490		490
(19)	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	2213		2213
(13)(19)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	165		165
(13)(19)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	33		33
(19)	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	440		440
(13)(19)	716-02.08 PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.	3423	1233	4656
(13)(19)	716-02.11 PLASTIC PAVEMENT MARKING (6" DOTTED LINE)	L.F.	1938	1190	3128
(13)(19)	716-02.15 PLASTIC PAVEMENT MARKING (U TURN ARROW)	EACH	8		8
(19)	716-04.05 PLASTIC PAVEMENT MARKING (STRAIGHT ARROW)	EACH	8		8
(13)(19)	716-04.06 PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH		2	2
(13)(19)	716-04.12 PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.		492	492
(19)	716-04.14 PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH		1	1
(20)(21)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	28.2		28.2
(21)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	28.2		28.2
	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	6458		6458
	717-01 MOBILIZATION	LS	0.9		0.9

FOOTNOTES	
(1)	NO DIRT OR DEBRIS WILL BE LEFT ON THE SHOULDERS. SHOULDERS SHOULD BE CLIPPED AS DIRECTED BY THE ENGINEER TO ALLOW FOR PROPER DRAINAGE. THIS WORK TO BE PERFORMED BEFORE ALL OTHER OPERATIONS.
(2)	TO BE USED AS DIRECTED BY THE ENGINEER.
(3)	INCLUDES 100 TONS FOR GENERAL SPOT LEVELING FOR NORTHBOUND LANES, AND 64 TONS FOR SPOT REPAIR FROM L.M. 11.25 TO 11.29 RT (SEE TABLE).
(4)	INCLUDES 107 TONS FOR MAINLINE, 20 TONS FOR RAMPS, AND 4 TONS FOR SIDE ROADS AND MEDIAN CROSSTOVERS.
(5)	INCLUDES 16276 TONS FOR MAINLINE, 2300 TONS FOR RAMPS AND MEDIAN CROSSTOVERS, AND 83 TONS FOR SIDE ROADS
(6)	INCLUDES 16123 TONS FOR MAINLINE, 2200 TONS FOR RAMPS AND MEDIAN CROSSTOVERS, 82 TONS FOR SIDE ROADS, AND 60 TONS FOR SPOT REPAIR FROM L.M. 11.25 TO 11.29 RT (SEE TABLE).
(7)	EARTH PAD TO BE CONSTRUCTED OF AGGREGATE.
(8)	CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
(9)	IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING METHOD APPROVED BY THE ENGINEER. TERMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 712-06 (CONSTRUCTION).
(10)	CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING OF CONSTRUCTION SIGNS.
(11)	TO BE USED TO REPLACE EXISTING DELINEATORS.
(12)	TO BE INSTALLED AT L.M. 10.035, 10.053, 11.828, 13.026, AND 13.032
(13)	SEE PAVEMENT MARKING IMPROVEMENTS SHEETS.
(14)	TO BE INSTALLED AT L.M. 11.015 LT.
(15)	TO BE INSTALLED AT L.M. 11.364 LT.
(16)	TO BE INSTALLED AT L.M. 13.747 RT.
(17)	TO BE INSTALLED AT L.M. 10.572 LT.
(18)	TO BE INSTALLED AT L.M. 10.612 RT.
(19)	CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FO R THERMOPLASTIC.
(20)	FOR TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS.
(21)	INCLUDES 22.0 L.M. FOR SOLID EDGELINES, 2.8 L.M. FOR DASHED LANE LINES, AND 3.4 L.M. FOR RAMPS.

NO UTILITY ADJUSTMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2

REV. 4-9-25: REVISED ITEM NOS. 712-01 AND 717-01. REVISED FOOTNOTE (1).

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04-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

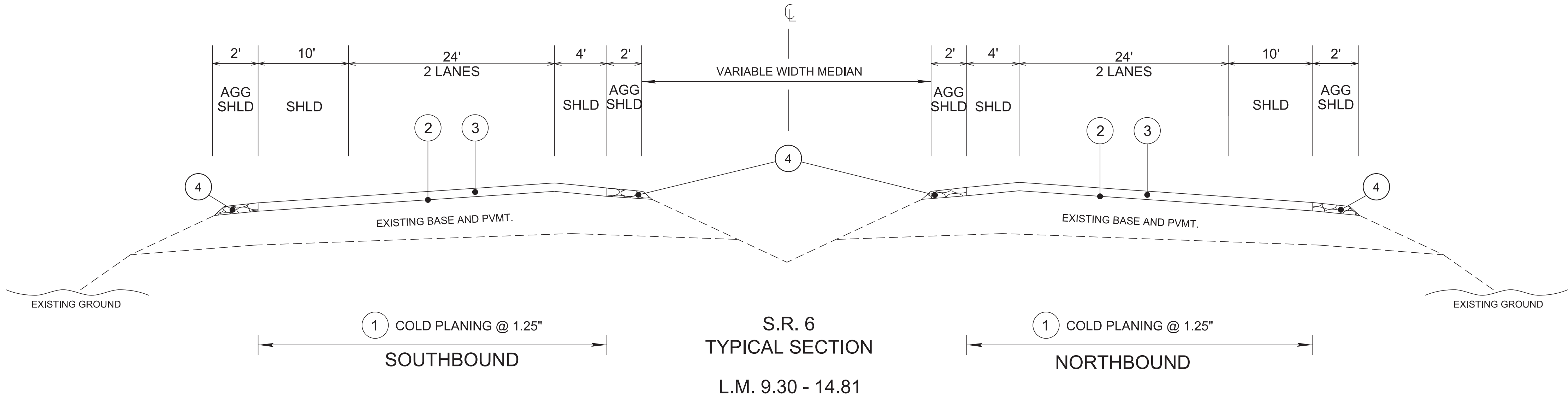
ESTIMATED
ROADWAY
QUANTITIES

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\\TDOT03NAS002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Maury Co. SR-6 L.M. 9.30 - 14.81\129517-00 002A Typical Sections and Pavement Schedule.sht

NOTE: ALL SIDE ROADS SHALL BE PAVED IN ACCORDANCE WITH TDOT GUIDELINES:

"PUBLIC ROAD INTERSECTIONS WILL BE RESURFACED A PAVER WIDTH THROUGH THE INTERSECTION AT A MINIMUM. ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS WILL BE RESURFACED TO THE END OF RADIUS."

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2A



PROPOSED PAVEMENT SCHEDULE

① COLD PLANING @ 1.25" THICK (APPROX. 131.25 LB./S.Y.)

415-01.01 COLD PLANING BITUMINOUS PAVEMENT

② TACK COAT

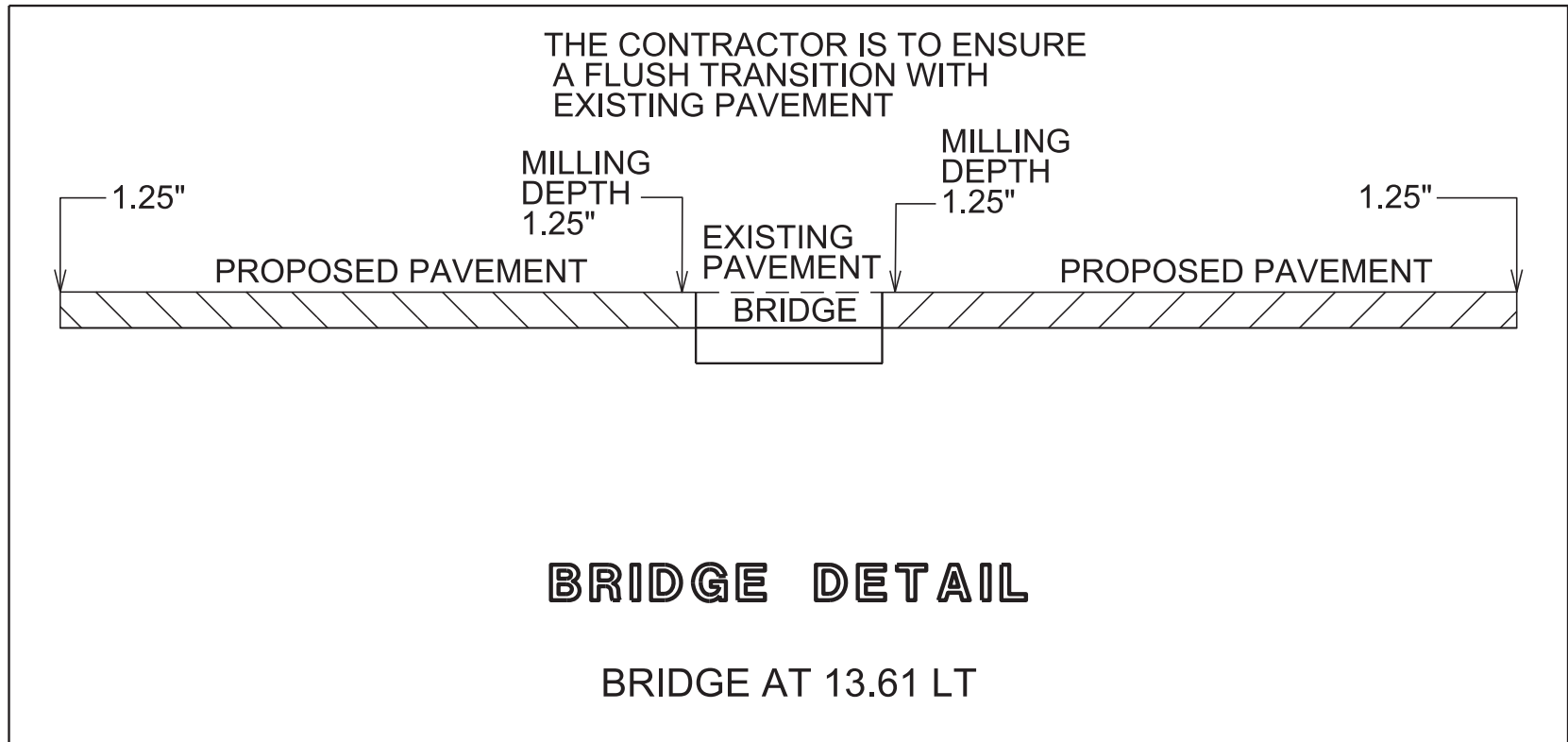
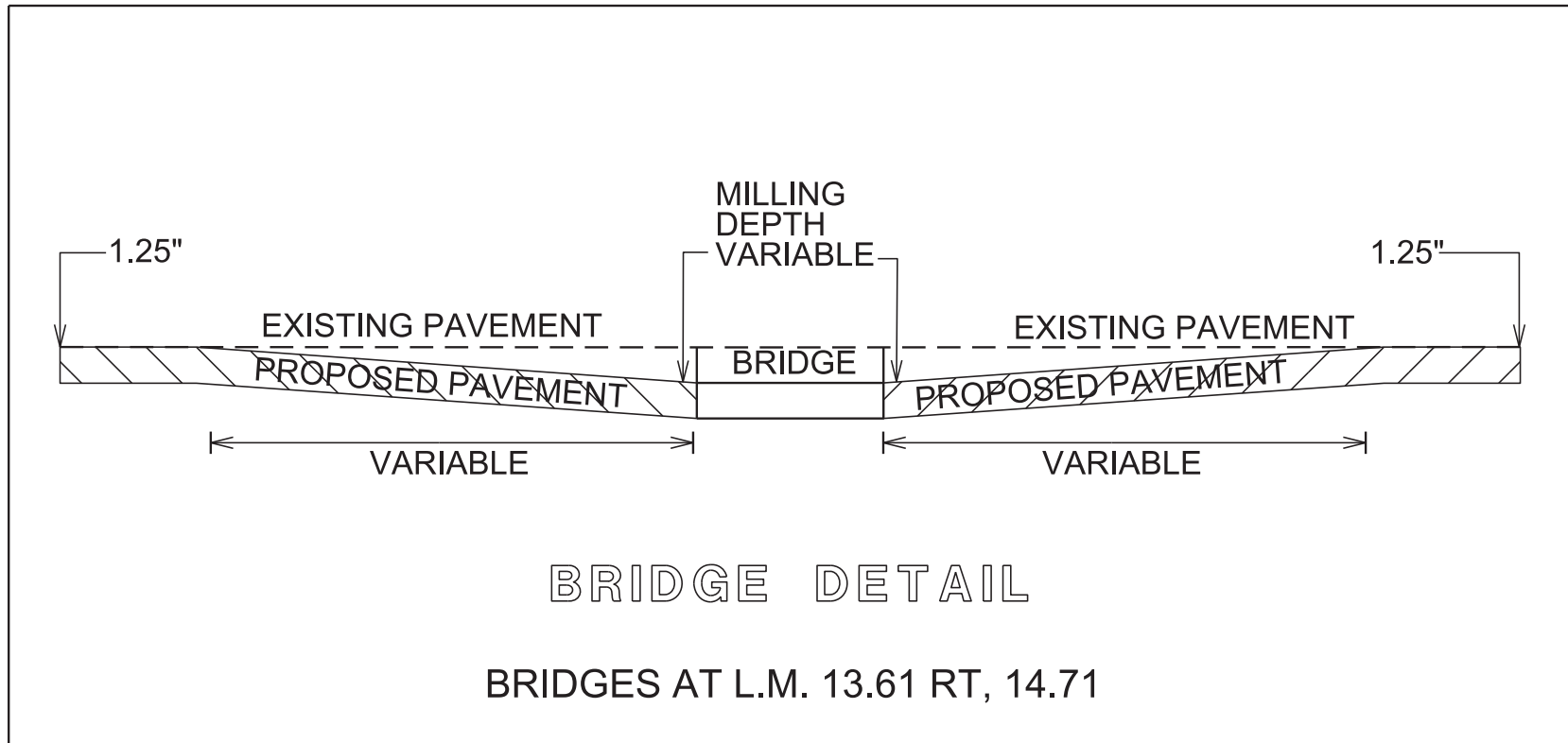
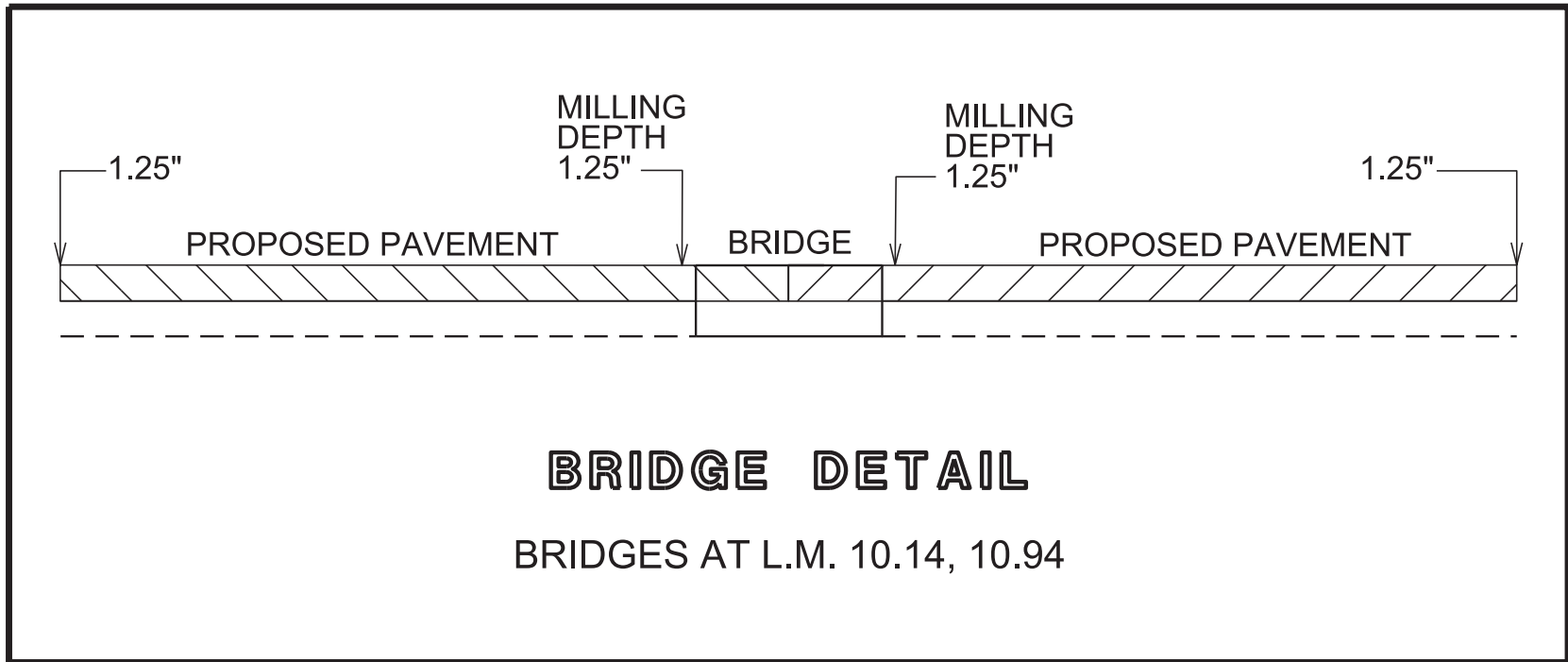
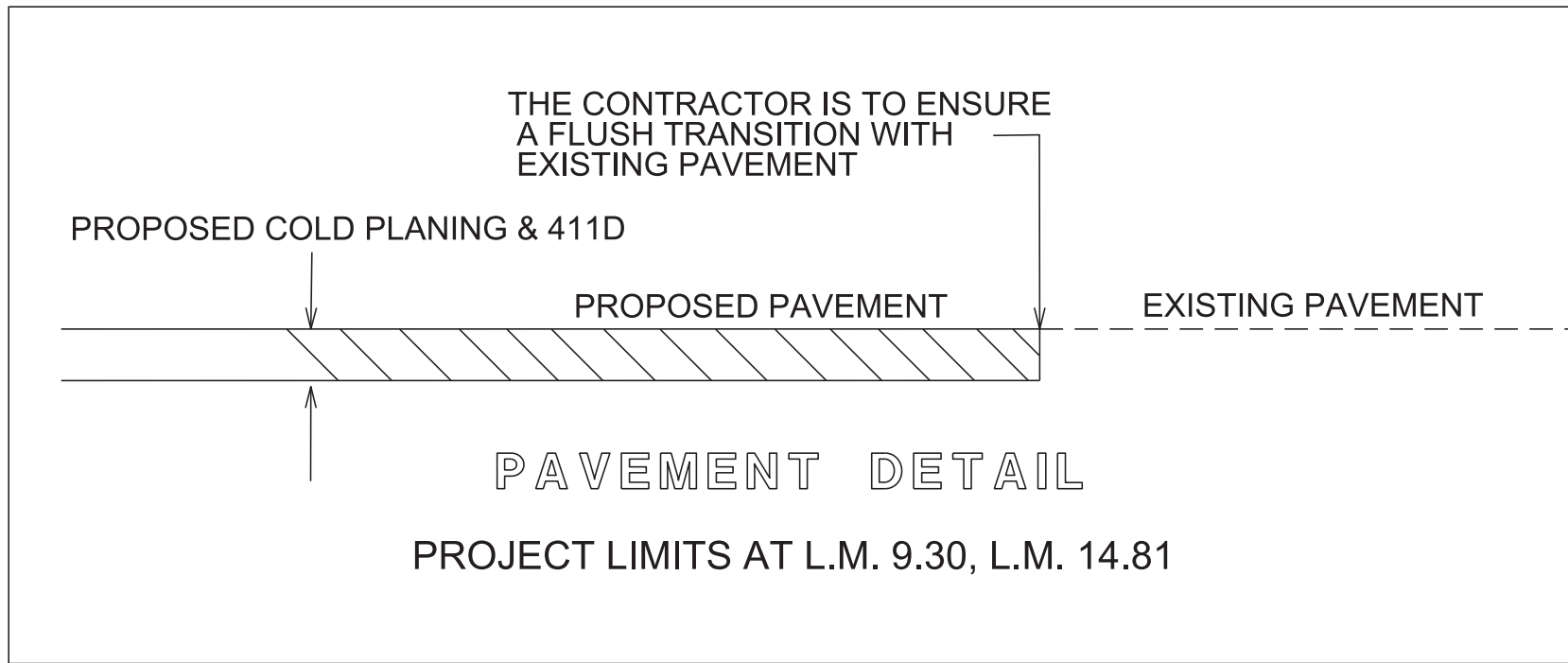
403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC).
SEE 403.05 FOR DETERMINING APPLICATION RATE IN FIELD

③ ASPHALTIC CONCRETE SURFACE (HOT MIX) PG70-22 GRADING "D" SURFACE @ 1.25" THICK (APPROX. 132.5 LB./S.Y.)

411-02.10 GRADING "D" SURFACE (PG70-22)

④ MINERAL AGGREGATE @ 1.5" THICK

303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"



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04-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

10-APR-2025 15:22
\\TDOT03NAS002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Maury Co. SR-6 L.M. 9.30 - 14.81\129517-00 002B General Notes.dgn

GENERAL NOTES

GUARDRAIL

- (2) THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.
- (13) AFTER THE PERMANENT SIGN LOCATIONS HAVE BEEN STAKED, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE ONSTRUCTION FIELD OFFICE. PAYMENT FOR LOCATION AND STAKING SHOULD BE INCLUDED IN THE BID PRICE FOR OTHER ITEMS OF CONSTRUCTION. ANY RELOCATION REQUIRED, DUE TO THE SIGN NOT BEING INSTALLED IN THE CORRECT LOCATION, WILL BE DONE AT THE CONTRACTOR’S EXPENSE.


CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2B

REV. 4-9-25: REMOVED NOTE (5).
ADDED NOTE (6).

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04-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

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SPECIAL NOTES

MISCELLANEOUS

- (1) THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN WORKING AROUND EXISTING GUARDRAIL, HIGHWAY SIGNS, OR ANY DEPARTMENT OF TRANSPORTATION STRUCTURE TO AVOID DAMAGE DURING THE COURSE OF CONSTRUCTION. ANY DAMAGE WILL BE REPLACED OR REPAIRED AT HIS OWN EXPENSE.

PAVEMENT

PAVING

- (2) SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (3) ALL STRUCTURES (DRAINAGE STRUCTURES, MANHOLES, UTILITY BOXES, ETC.) SHALL HAVE A FINISHED GRADE COMPATIBLE WITH THE FINISHED GRADE OF THE ROADWAY.

RESURFACING

- (6) AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING TRAFFIC ONTO EXCAVATED ROADWAY.

PAVEMENT MARKINGS

- (5) THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (6) THE CONSTRUCTION SIGNS, FLAGMEN, AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (7) ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED ON ALL RAMPS AND SIDE ROADS PRIOR TO MERGING WITH THE MAINLINE. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTOR'S SUPERINTENDENT.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (8) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

A. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

B. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

C. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

D. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

WORK RESTRICTIONS

- (9) CONSTRUCTION ACTIVITIES SHALL NOT INTERFERE WITH SCHOOL OR WORK TRAFFIC EITHER AM OR PM OR AS DIRECTED BY THE ENGINEER.


TSRR RAILROAD CROSSING NOTES

- (1) THE CONTRACTOR SHALL REPLACE ALL RR ADVANCE WARNING SIGNS, RXR PAVEMENT MARKINGS AND STOP LINES AT APPROACHES TO THIS GRADE CROSSING. THEY MUST SATISFY THE 2009 MUTCD STANDARDS AND REQUIREMENTS
- (2) ANY PAVEMENT PREPARATION, PAVEMENT INSTALLATION, PAVEMENT MARKINGS AND/OR RAILROAD WARNING SIGN INSTALLATION WITHIN 25- FEET OF THE NEAREST TENNESSEE SOUTHERN RR TRACK UTILIZING CONTRACTOR EQUIPMENT OR PERSONNEL WILL REQUIRE FULL TIME RAILROAD FLAGGING SERVICES OR AS OTHERWISE DETERMINED BY THE RAILROAD ONLY.
- (3) SEE AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C FOR WORK UP TO AND NEAR THE TENNESSEE SOUTHERN RR GRADE CROSSING REGARDING FLAGGING RULES AND INSURANCE REQUIREMENTS.
- (4) COLD PLANING AS CLOSE AS PRACTICAL TO THE FIELD SIDES OF THE CROSSING SURFACE, FOLLOWED BY RESURFACE UP TO AND LEVEL WITH THE TOP OF THE FIELD SIDES OF THE CROSSING SURFACE.
- (5) THE CROSSING SURFACE SHOULD BE ON THE SAME PLANE AS TENNESSEE SOUTHERN RR FOR A DISTANCE OF 30 INCHES OUTSIDE THE RAILS.
- (6) THE SURFACE OF THE HIGHWAY SHALL NOT BE MORE THAN 3 INCHES HIGHER OR LOWER THAN THE TOP OF THE TENNESSEE SOUTHERN RR AT A POINT 30 FEET FROM THE RAIL, UNLESS SUPERELEVATION MAKES A DIFFERENT LEVEL APPROPRIATE.
- (7) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE TENNESSEE SOUTHERN RR 'S TRACK FACILITIES AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY TENNESSEE SOUTHERN RR SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER. THE CONTRACTOR SHALL NOT STORE ANY OF HIS CONSTRUCTION EQUIPMENT ON THE RAILROAD'S RIGHT-OF-WAY. THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVE OF THE RAILROAD:

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2C

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SPECIAL
NOTES

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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4)

ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5)

SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6)

THIS PROJECT INCLUDES COLD PLANING, RESURFACING, PAVEMENT MARKINGS, SIGNING, BRIDGE REPAIR, AND SAFETY IMPROVEMENTS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1)

IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6)

EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

RAILROAD ENVIRONMENTAL

- (15)

THE CONTRACTOR SHALL MAINTAIN A COMPLETE AND COMPREHENSIVE PLAN TO PREVENT ROADWAY AND/OR CONSTRUCTION SEDIMENT OR DEBRIS AND ANY PETROLEUM-BASED PRODUCTS OR CHLORINATED SOLVENTS, PAINTS OR COATINGS ETC. FROM FALLING ONTO THE RAILROAD'S RIGHT-OF-WAY AND/OR FROM ENTERING THE DRAINAGE DITCHES OR DRAINAGE STRUCTURES OF THE RAILROAD, AND ANY SEDIMENT OR DEBRIS OR PETROLEUM-BASED PRODUCTS OR CHLORINATED SOLVENTS, ETC. THAT DO ENTER SUCH DRAINAGE AREAS OF THE RAILROAD'S RIGHT-OF-WAY ARE TO BE REMOVED IN ACCORDANCE WITH RULES SET FORTH BY TSRR AND AT THE CONTRACTOR'S EXPENSE.

HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

- (37)

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)

OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44)

ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45)

FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46)

APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47)

ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48)

THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49)

IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50)

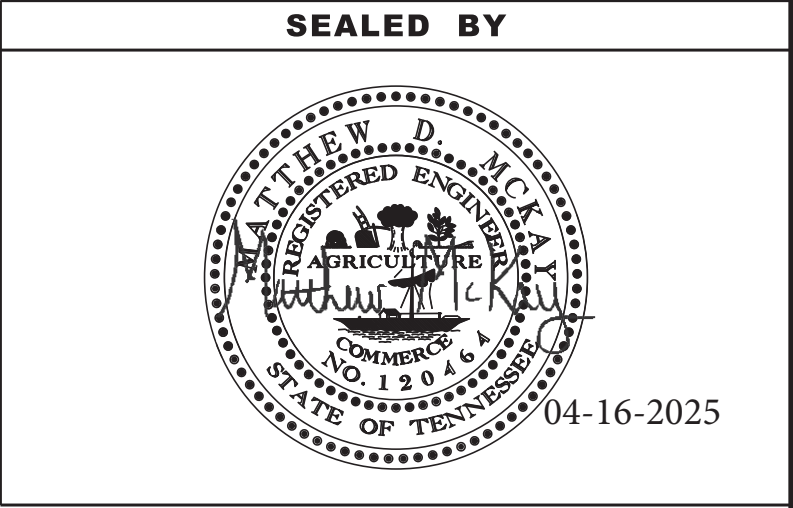
FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51)

IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52)

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53)

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2D



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2E

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L x W	S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.	
E5-1	EXIT	72" X 60"	30	3	90	
E5-2	EXIT OPEN	48" X 36"	12	3	36	
G20-1	ROAD WORK NEXT 6 MILES	48" X 24"	8	4	32	
G20-2	END ROAD WORK	48" X 24"	8	14	112	
R1-2	YIELD	48" X 48"	16	3	48	
W3-2	YIELD AHEAD	48" X 48"	16	3	48	
W4-2L	LEFT LANE ENDS SYMBOL - PORTABLE	48" X 48"	16	2	32	
W4-2R	RIGHT LANE ENDS SYMBOL - PORTABLE	48" X 48"	16	2	32	
W8-11	UNEVEN LANES	48" X 48"	16	30	480	
W8-15	GROOVED PAVEMENT	48" X 48"	16	30	480	
W8-15P	MOTORCYCLE (PLAQUE)	30" X 24"	5	30	150	
W20-1	ROAD WORK 1 MILE	48" X 48"	16	4	64	
W20-1	ROAD WORK 1/2 MILE	48" X 48"	16	4	64	
W20-1	ROAD WORK 1000 FT	48" X 48"	16	4	64	
W20-1	ROAD WORK AHEAD	48" X 48"	16	10	160	
W20-5L	LEFT LANE CLOSED 1/2 MILE - PORTABLE	48" X 48"	16	2	32	
W20-5L	LEFT LANE CLOSED 1500 FT - PORTABLE	48" X 48"	16	2	32	
W20-5R	RIGHT LANE CLOSED 1/2 MILE - PORTABLE	48" X 48"	16	2	32	
W20-5R	RIGHT LANE CLOSED 1500 FT - PORTABLE	48" X 48"	16	2	32	
W21-2	FRESH OIL - PORTABLE	48" X 48"	16	2	32	
				TOTAL	2052	


BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
60SR0060033	10.14 RT	SUGAR FORK CREEK	229.25'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT
60SR0060034	10.14 LT	SUGAR FORK CREEK	229.25'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT
60SR0060035	10.94	TSRR	136'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT
60SR0060071	12.30	BRANCH	22'	PAVE WITH PLANS MIX/TREATMENT TYPE
60SR0060077	13.61 RT	ZION RD	98'	REMOVE ASPHALT (IF PRESENT). SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
60SR0060078	13.61 LT	ZION RD	98'	PROVIDE DECK REPAIR ITEM. SEAL WITH TYPE 1 THIN EPOXY OVERLAY. REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
60SR0060037	14.71 RT	SOUTHERN RR	233.55'	REMOVE ASPHALT (IF PRESENT). SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
60SR0060038	14.71 LT	SOUTHERN RR	233.55'	REMOVE ASPHALT (IF PRESENT). SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)

SPECIAL NOTE:
THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

PROPOSED GUARDRAIL (RESURFACING)										
SIDE				LOG MILE	GUARDRAIL		EARTH PAD		TERMINAL ANCHORS	
					GUARDRAIL TRANSITION 27 IN TO 31 IN (28.125') 705-02.10 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)	EARTH PAD FOR TYPE 38 G.R. END TREATMENT 705-04.09 (EACH)	EARTH PAD FOR GUARDRAIL END TREATMENT 705-04.10 (EACH)	TYPE 38 MASH TL3 (46.875') 705-06.20 (EACH)	TYPE 21 MASH TL2 (21.875') 705-06.30 (EACH)
LT	LT MEDIAN	RT MEDIAN	RT							
		X		10.129	1	75	1		1	
X				RAMP @ L.M. 10.85	1	75		1		1
			X	RAMP @ L.M. 10.86	1	75		1		1
			X	14.362	1	75	1		1	
TOTALS					4	300	2	2	2	2

SPOT REPAIR								
LOCATION			LENGTH	WIDTH	DEPTH	B-M2 MIX 307-02.08 (TON)	COLD PLANING 415-01.01 (TON)	
BEG. L.M.	END L.M.	SIDE LT / SB RT / NB						
11.25	11.29		X	211.2'	24'	2"	64	60
TOTALS							64	60

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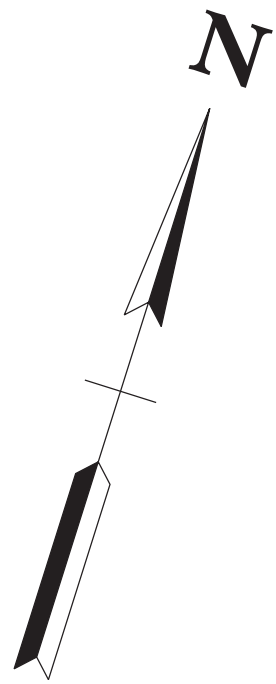
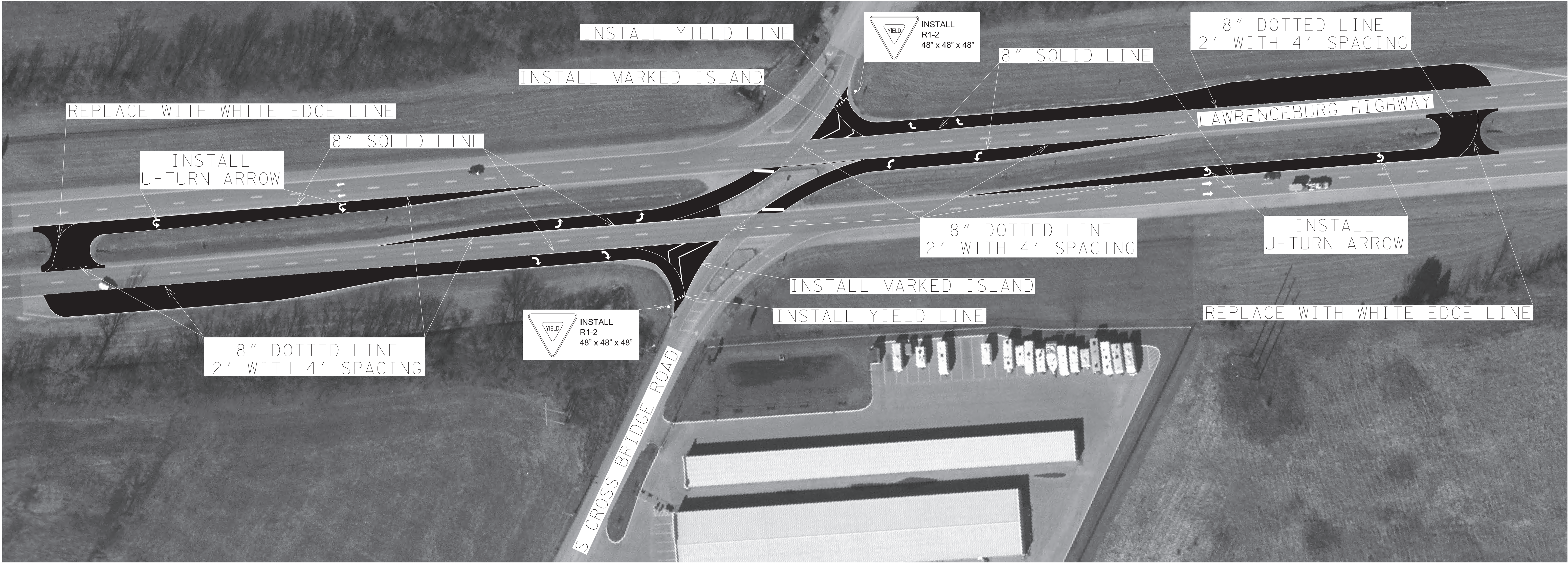


04-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F



STATE ROUTE 6
9.314 LOG MILE TO 9.620 LOG MILE
MAURY COUNTY

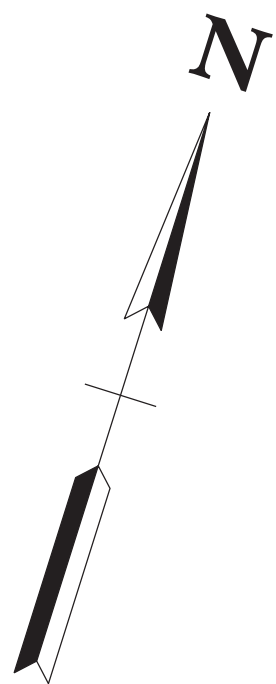
REFERENCE STANDARD DRAWINGS
T-M-3, T-M-4, AND T-M-19

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STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F2



STATE ROUTE 6
10.735 LOG MILE TO 10.978 LOG MILE
MAURY COUNTY

REFERENCE STANDARD DRAWINGS
T-M-6

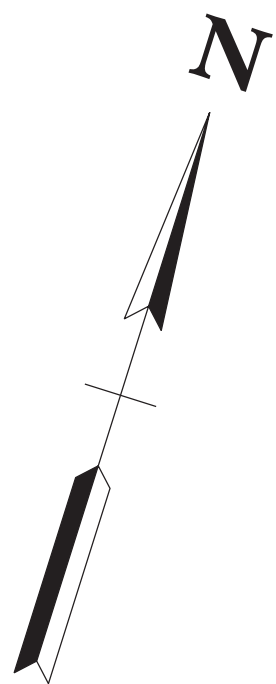
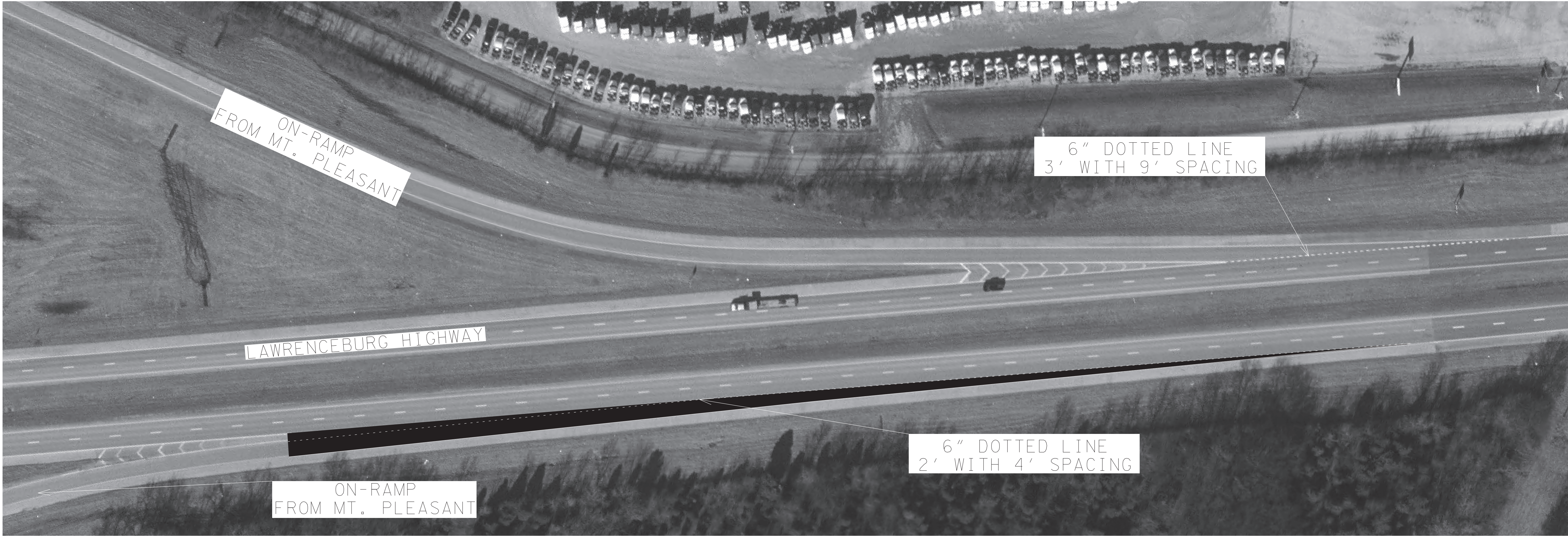
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DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F3



STATE ROUTE 6
10.978 LOG MILE TO 11.296 LOG MILE
MAURY COUNTY

REFERENCE STANDARD DRAWINGS
T-M-6

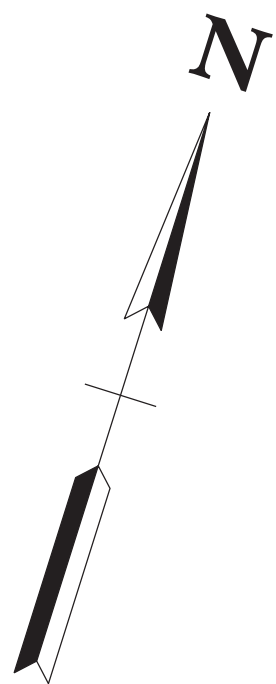
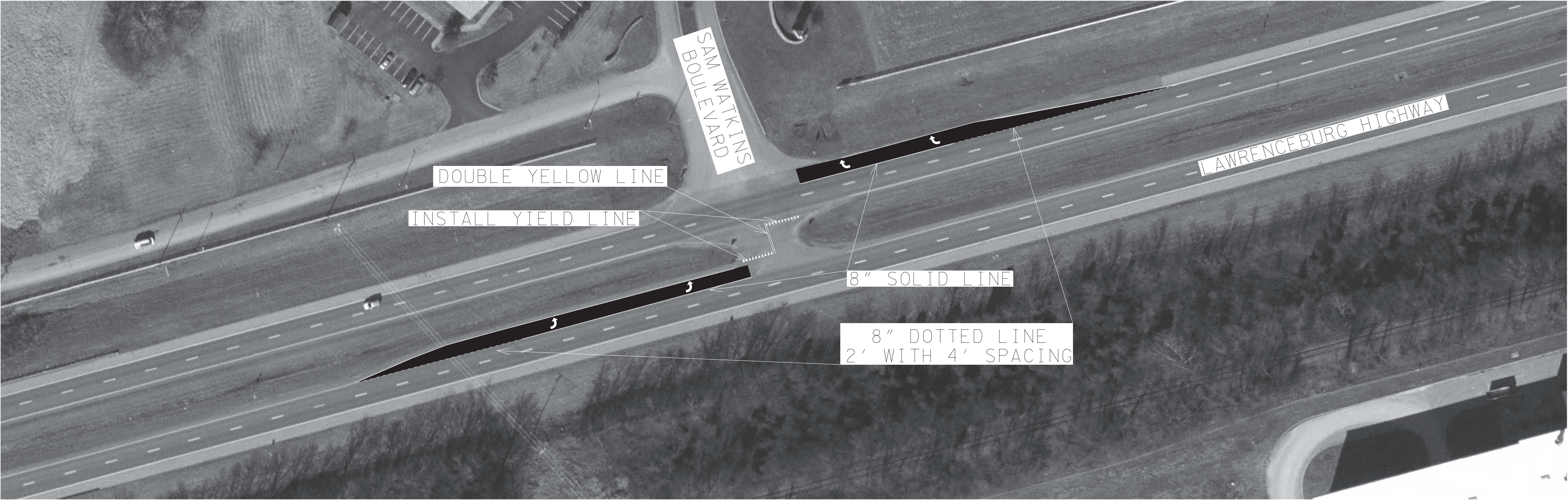
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PAVEMENT
MARKING
IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F4



STATE ROUTE 6
11.698 LOG MILE TO 11.866 LOG MILE
MAURY COUNTY

REFERENCE STANDARD DRAWINGS
T-M-4

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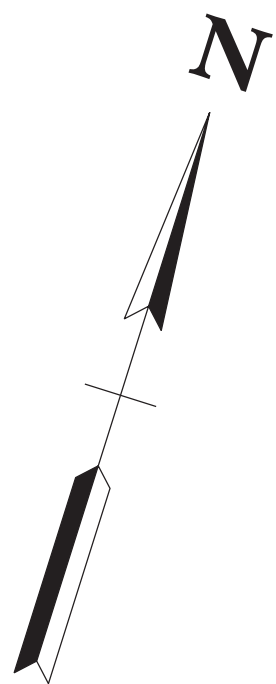
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PAVEMENT
MARKING
IMPROVEMENTS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F5



STATE ROUTE 6
12.576 LOG MILE
MAURY COUNTY

REFERENCE STANDARD DRAWINGS
T-M-4

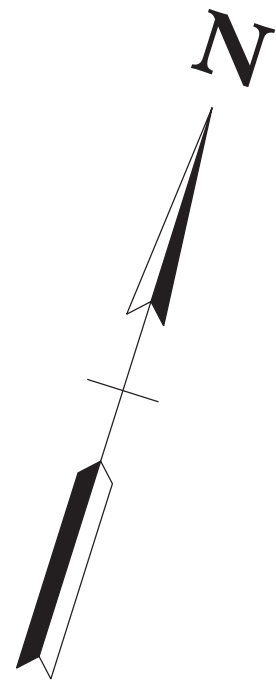
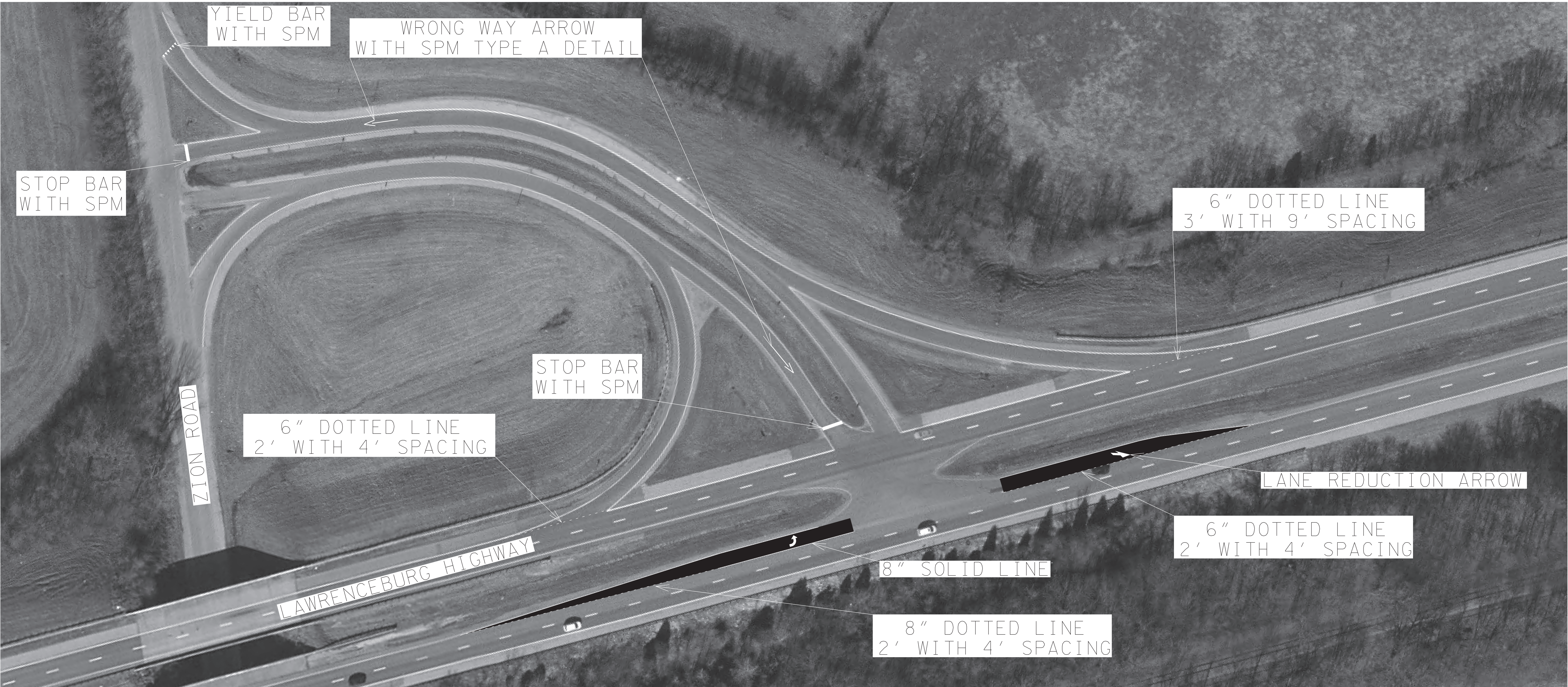
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DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F6



STATE ROUTE 6
13.732 LOG MILE
MAURY COUNTY

REFERENCE STANDARD DRAWINGS
T-M-4, T-M-6, T-M-9A, AND T-M-9B

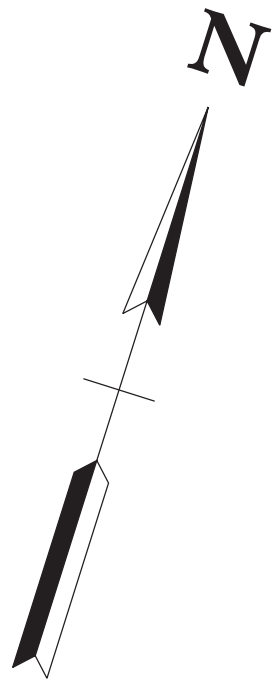
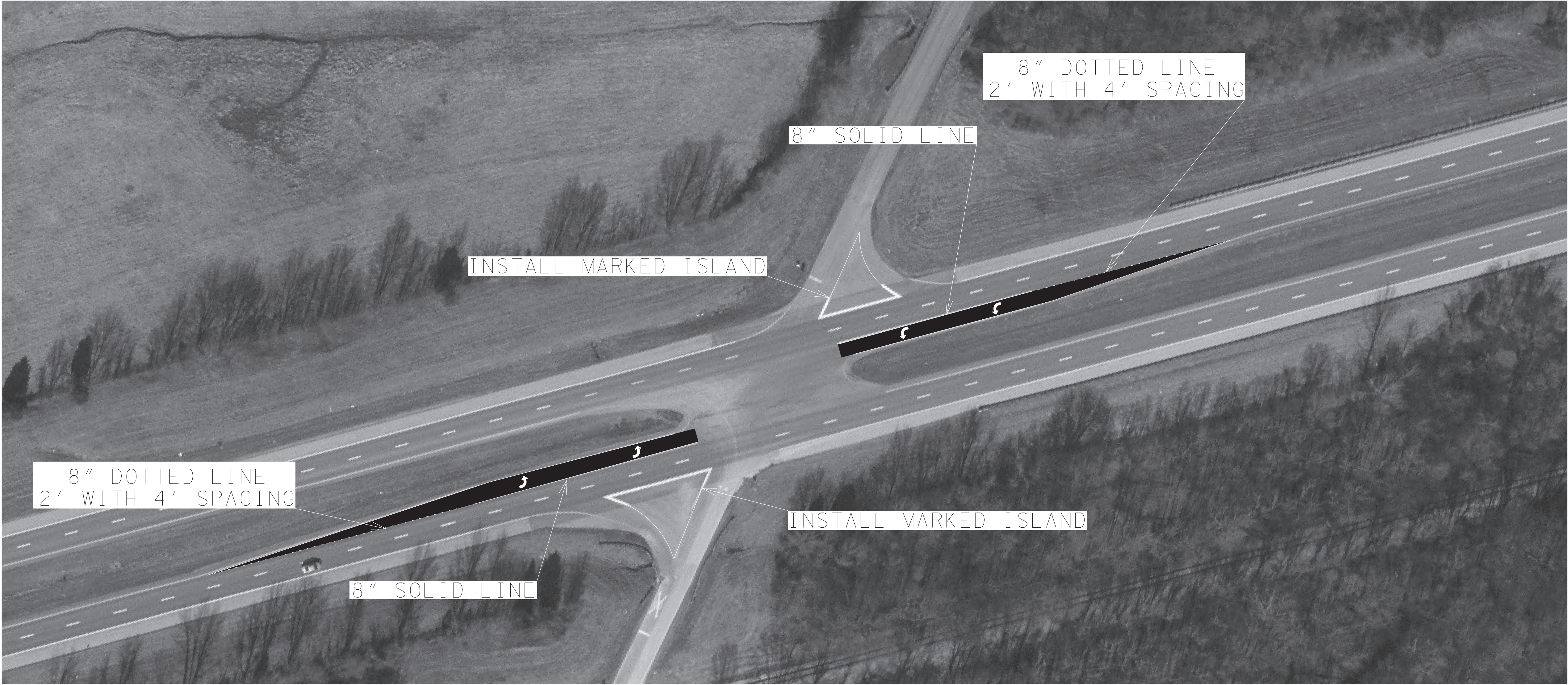
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	2F7



STATE ROUTE 6
14.283 LOG MILE AND 14.458 LOG MILE
MAURY COUNTY

REFERENCE STANDARD DRAWINGS
T-M-3 AND T-M-4

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

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RIGHT-OF-WAY

- (1) EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

UTILITIES

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

WATER:
COLUMBIA WATER
201 PICKENS LANE
COLUMBIA, TN 38401
JONATHAN HARDIN
JONATHAN.HARDIN@CPWS.COM
O: 931-375-7646

TELEPHONE:
AT&T
116 SOUTH CANNON AVENUE
MURFREESBORO, TN 37129
KENNETH LEE KORNEGAY
KK4096@ATT.COM
O: 615-848-2082

GAS:
ATMOS ENERGY
810 CRESCENT CENTER DRIVE,
SUITE 600
FRANKLIN, TN 37067
TERI DOSS
TERI.DOSS@ATMOSENERGY.COM
O:615-418-6699

ROBERT ARNOLD
ROBERT.ARNOLD@ATMOSENERGY.COM
O:615-771-8311
C:615-310-3020

HORTON HIGHWAY UTILITY DISTRICT
102 S. MAIN STREET P.O. BOX 85
CHAPEL HILL, TN 37034
MARK CURTIS
UTILITYCO@UNITED.NET
O: 931-364-2283

BP PIPELINES
30 SOUTH WACKER DRIVE
CHICAGO, IL 60606
KEITH BOYLE
BPPIPELINESROW@BP.COM
O: 312-809-4708
C: 312-358-0711

SEWER:
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1244 TREATMENT PLANT RD.
COLUMBIA, TN 38401
DONNIE BOSHERS
DBOSHERS@COLUMBIATN.COM
O: 931-560-1001 (EXT. 1011)

WATER, SEWER, GAS:
MT. PLEASANT UTILITIES
100 PUBLIC SQUARE
MT. PLEASANT, TN 38474
TED HOWELL
THOWELL@MTPLEASANT-TN.GOV
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C: 931-774-4917

DALE BROWN
DBROWN@MTPLEASANT-TN.GOV
C:931-774-6343

ELECTRIC:
MT. PLEASANT POWER SYSTEMS
123 NORTH MAIN STREET
MT. PLEASANT, TN 38474
AL KERSTIENS
AKERSTIENS@MOUNTPLEASANTPOWE
R.COM
O: 931-379-3233
C: 931-626-6109

DUCK RIVER ELECTRIC MEMBERSHIP CORP.
1411 MADISON STREET
SHELBYVILLE, TN 37160
SCOTT DAHLSTROM
SDAHLSTROM@DREMC.COM
O: 931-680-5840
C: 931-703-0248

TENNESSEE VALLEY AUTHORITY
1101 MARKET STREET MR-4G
CHATTANOOGA, TN 37402-2801
STEPHEN WILLIAMS
SEWILLIAMS@TVA.GOV
C: 662-255-6272

COLUMBIA POWER
201 PICKENS LANE
COLUMBIA, TN 38401
RYAN MASSEY
RYAN.MASSEY@CPWS.COM
O: 931-375-7742

FOC:
COLUMBIA BROADBAND
201 PICKENS LANE
COLUMBIA, TN 38401
BRAD TEBBEN
BRAD.TEBBEN@CPWS.COM
O: 931-375-7682


LUMEN (FORMERLY CENTURYLINK & LEVEL 3 COMMUNICATIONS)
520 WHALEY STREET
COLUMBIA, SC 29201
RUSS WHEAT
RUSS.WHEAT@LUMEN.COM
RELOCATIONS@LUMEN.COM
O: 803-832-2576
C: 803-206-9563

2530 PERIMETER PLACE DRIVE
NASHVILLE, TN 37214
JEFFREY CANNON
JEFFREY.CANNON@LUMEN.COM
O:615-263-1128
C: 615-419-6617

CABLE:
CHARTER COMMUNICATIONS
1757 NORTH POINTE DR.
COLUMBIA, TN 38401
MICHAEL OVERMAN
MICHAEL.OVERMAN@CHARTER.COM
O:931-548-8553 (EXT. 28553)
C:931-981-8078

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	3

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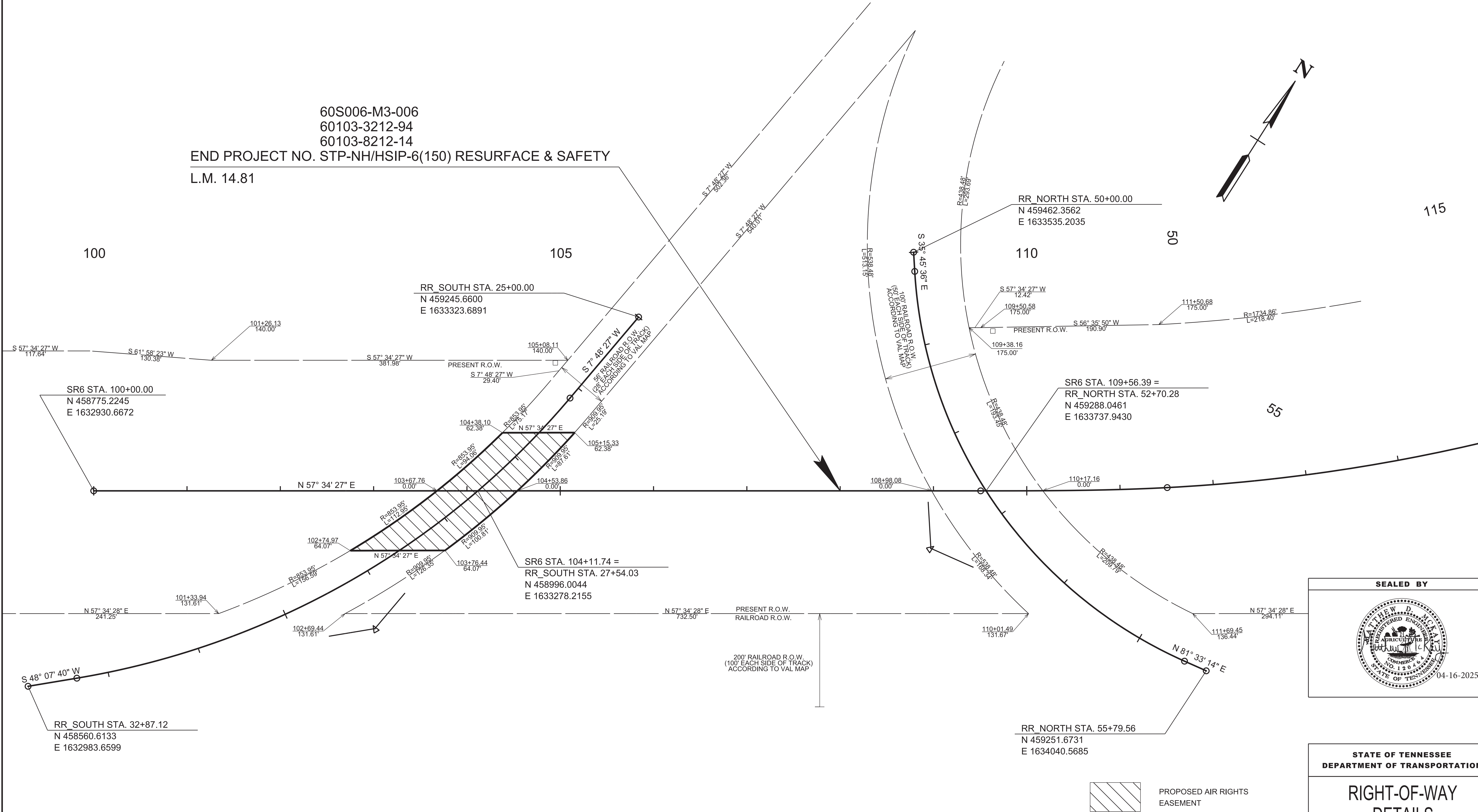
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION


RIGHT-OF-WAY NOTES,
UTILITY NOTES, AND
UTILITY OWNERS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP-NH/HSIP-6(150)	4A

60S006-M3-006
60103-3212-94
60103-8212-14
END PROJECT NO. STP-NH/HSIP-6(150) RESURFACE & SAFETY
L.M. 14.81



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04-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY
DETAILS

STA. 100+00 TO STA. 115+00
SCALE: 1"=50'

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THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME _____ **SHEET NO.** _____
 SIGNATURE SHEET _____ STRUCTURE-SIGN
 BRIDGE PLANS _____ B1 THRU B10

	STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
	SIGNATURE SHEET

\$\$\$SYTIME\$\$\$\$\$
 \$\$\$DGNPES\$\$\$\$\$



THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME SIGNATURE SHEET _____ **SHEET NO.** STRUCTURE-SIGN 2
 BRIDGE PLANS _____ B1 - B10

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	12-23-24	BE	ADDED SIGNATURE-SIGN 2

YEAR	PROJECT NO.	SHEET NO.
2025	60S006-M3-006	STRUCTURE-SIGN 2

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

 SIGNATURE SHEET

\$\$\$\$\$\$TIME\$\$\$\$\$\$
 \$\$\$\$\$\$GNDECE\$\$\$\$\$

①

LAST REV. DATE 1

1

LAYOUT OF THE BRIDGE	M-144-14
SUPERSTRUCTURE	M-144-17
LAYOUT OF THE BRIDGE	M-379-1
SUPERSTRUCTURE	M-379-5
LAYOUT OF THE BRIDGE	M-171-2
SUPERSTRUCTURE	M-171-6
LAYOUT OF THE BRIDGE	M-379-15
SUPERSTRUCTURE	M-379-19
REINFORCED CONCRETE PAVEMENT AT THE BRIDGE ENDS	STD-1-5

1

3-1

PIN NO.:	129517.00	DATE:	/ /
DESIGN BY:		DATE:	12/23
DRAWN BY:	SILESHI ERGICHO	DATE:	12/23
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /

①

** NOTE: PARTIAL DEPTH DECK REPAIRS
 AREAS WILL BE LOCATED AFTER
 THE EXISTING ASPHALT IS REMOVED.

BRIDGE DECK REPAIR MATERIAL

PATCHING MATERIAL SHOULD BE ABLE TO ACHIEVE A MINIMUM
3000PSI AT 18 HOURS.
PATCHING MATERIAL SHALL REACH 3000PSI BEFORE OPENING TO THE TRAFFIC.

NOTE: PARTIAL DEPTH REPAIRS
ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED.
CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL
SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR
OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE
BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

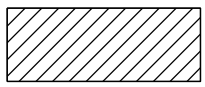
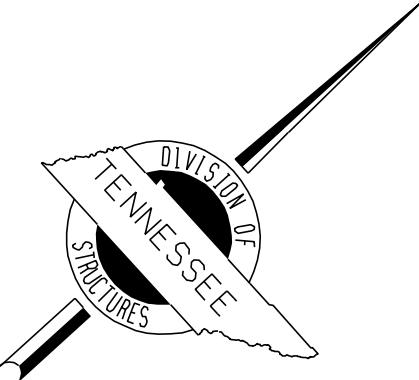
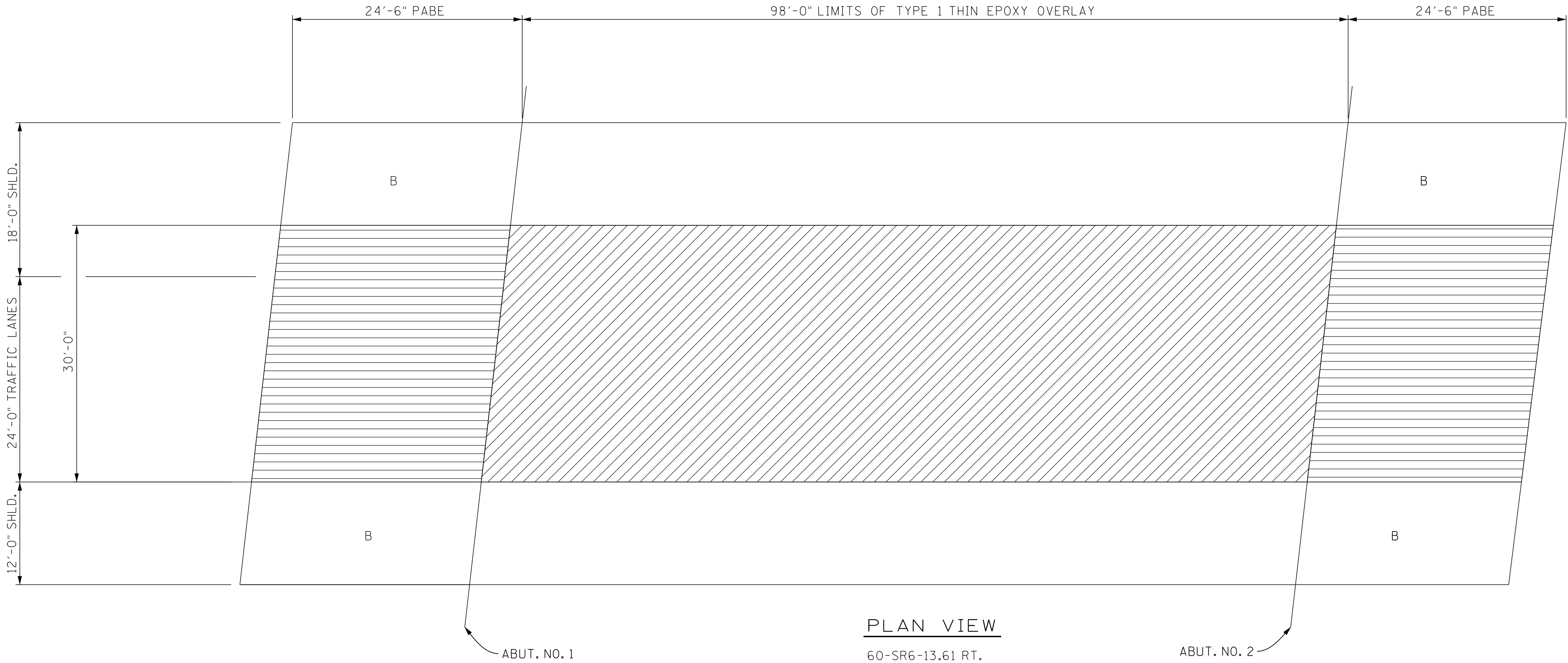
NOTE: ITEM NOS. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SHALL BE BID THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS APPROVED BY THE ENGINEER.

1

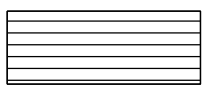
BRIDGE TABULATION,
ESTIMATED QUANTITIES,
REPAIRS NOTES AND DETAILS
60-SR6-13.61 RT. & LT.
OVER ZION ROAD
60-SR6-14.71 RT. & LT. OVER
TENNESSEE SOUTHERN
RAILROAD
MAURY COUNTY
2025

B-2

PROJECT NO.		YEAR	SHEET NO.
60S006-M3-006		2025	B-3
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1.	12-23-24	B.E.	CHANGED YEAR
	- -		
	- -		
	- -		
	- -		



DENOTES: APPROXIMATE AREAS OF ASPHALT TO BE REMOVED FROM BRIDGE DECK TO BE COVERED WITH TYPE 1 THIN EPOXY OVERLAY.



DENOTES: AREAS THAT HAVE EXISTING ASPHALT OVER APPROACH PAVEMENT SHALL BE PAVED, SEE PAVING SCHEDULE.

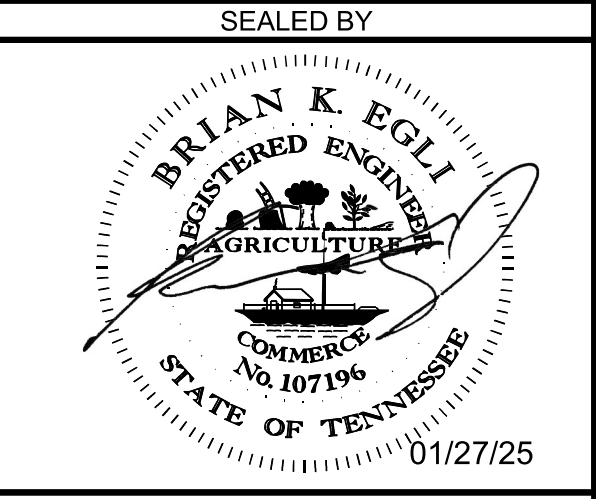
B

DENOTES: PAVEMENT AT SHOULDER TO BE PAVED EQUIVALENT DEPTH TO THE EXISTING ASPHALT.

NOTE: EXISTING ASPHALT ON THE BRIDGE DECK SHOULD BE REMOVED AND REPLACE WITH TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE). AVERAGE ASPHALT THICKNESS IS ± 2.0 ".

NOTE: COST OF REMOVING EXISTING ASPHALT FROM THE BRIDGE DECK SHALL BE INCLUDED IN ITEM NO. 617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.

NOTE: THE CONTRACTOR SHOULD TAKE EXTREME CARE SHALL BE TAKEN WHEN COLD PLANING THE EXISTING ASPHALT OFF THE BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING CONCRETE DECK. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED AREAS TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.

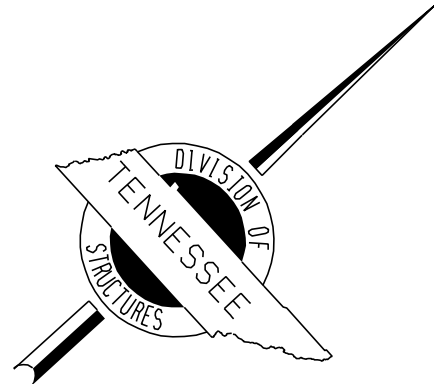


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
APPROXIMATE REPAIRS
LOCATIONS
60-SR6-13.61 RT.
OVER ZION ROAD
MAURY COUNTY
2025



PIN NO.:	129517.00	DATE:	/ /
DESIGN BY:	SILESHI ERGICHO	DATE:	12/23
DRAWN BY:	KEVIN MARTINKO	DATE:	12/23
SUPERVISED BY:		DATE:	/ /

①



60-SR6-13.61 LT.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
APPROXIMATE REPAIRS
LOCATIONS
60-SR6-13.61 LT.
OVER ZION ROAD
MAURY COUNTY
2025

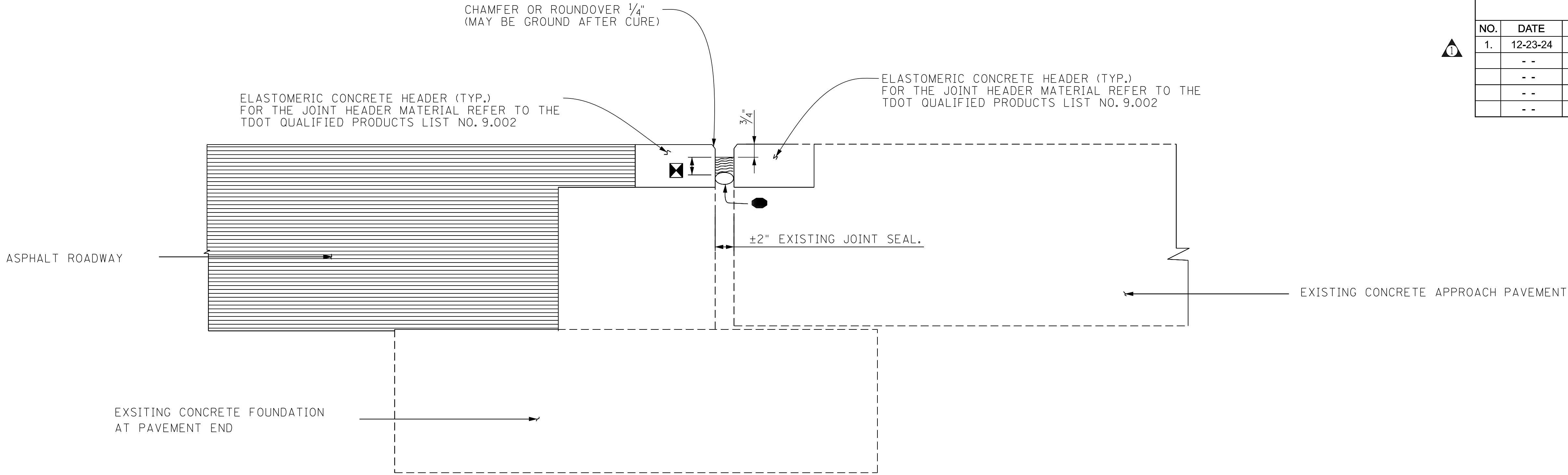
B - 4

PIN NO.:	129517.00	
DESIGN BY:		DATE: / /
DRAWN BY:	SILESHI ERGICHO	DATE: 12/23
SUPERVISED BY:	KEVIN MARTINKO	DATE: 12/23
CHECKED BY:		DATE: / /

\$\$\$SYTIME\$\$\$\$\$\$
\$\$\$DGN\$PEC\$\$\$

PIN NO.: 129517.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:

DATE: / /
DATE: 12/23
DATE: 12/23
DATE: / /



- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER,THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL,BACKER ROD SHALL BE AS PER JOINT MANUFACTURERS RECOMMENDATIONS.
- ☒ NOTE: FULL DEPTH OF ALL EXISTING JOINTS SHALL SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD PUOR TWO PART SILICONE SEALER FROM QPL 5.001.

EXPANSION JOINT REPAIR NOTES:

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM QPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM QPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE QPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

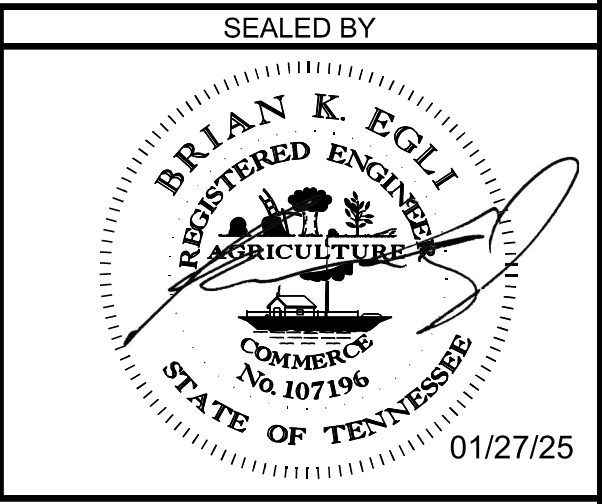
PRIOR TO THE INSTALLATION OF THE NEW JOINT,THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT,DEBRIS, AND PRIOR CONSTRUCTION MATERIAL,ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE,SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION,TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM,INSTALLING THE NEW JOINT SYSTEM,LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT,IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

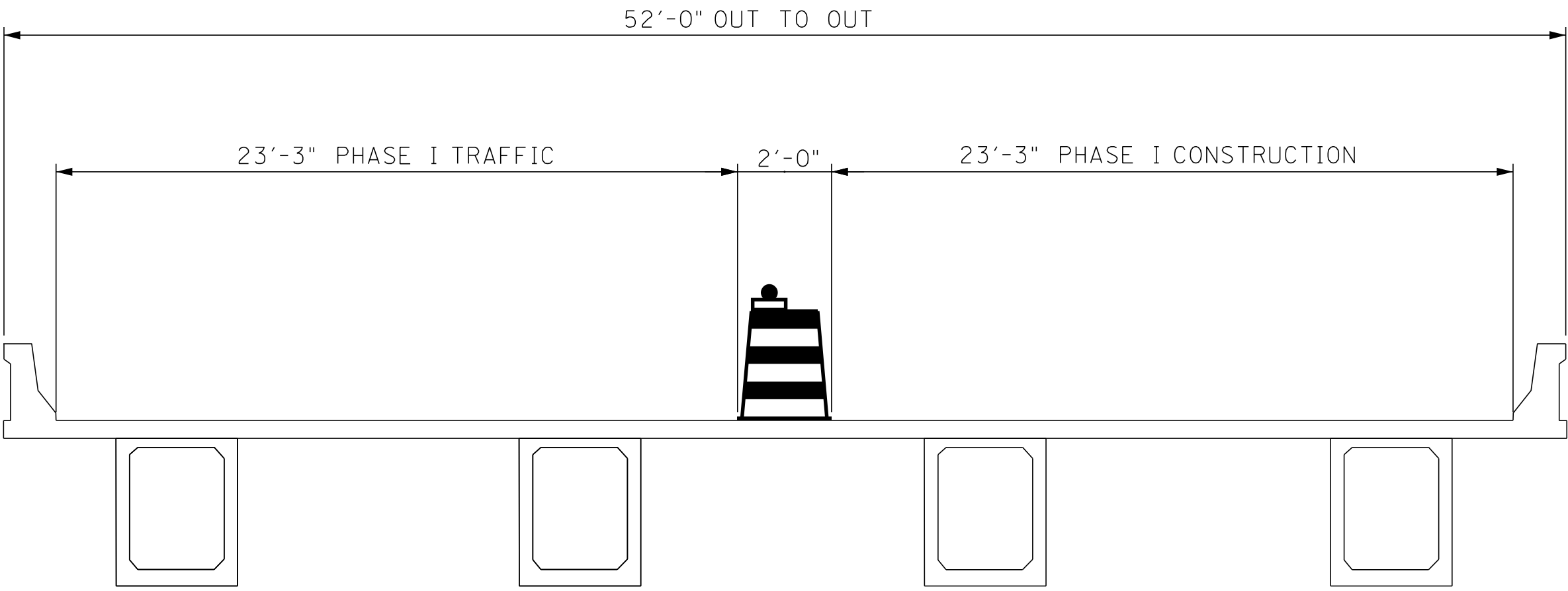
PROJECT NO.		YEAR	SHEET NO.
60S006-M3-006		2025	B-5
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1.	12-23-24	B.E.	CHANGED YEAR
	- -		
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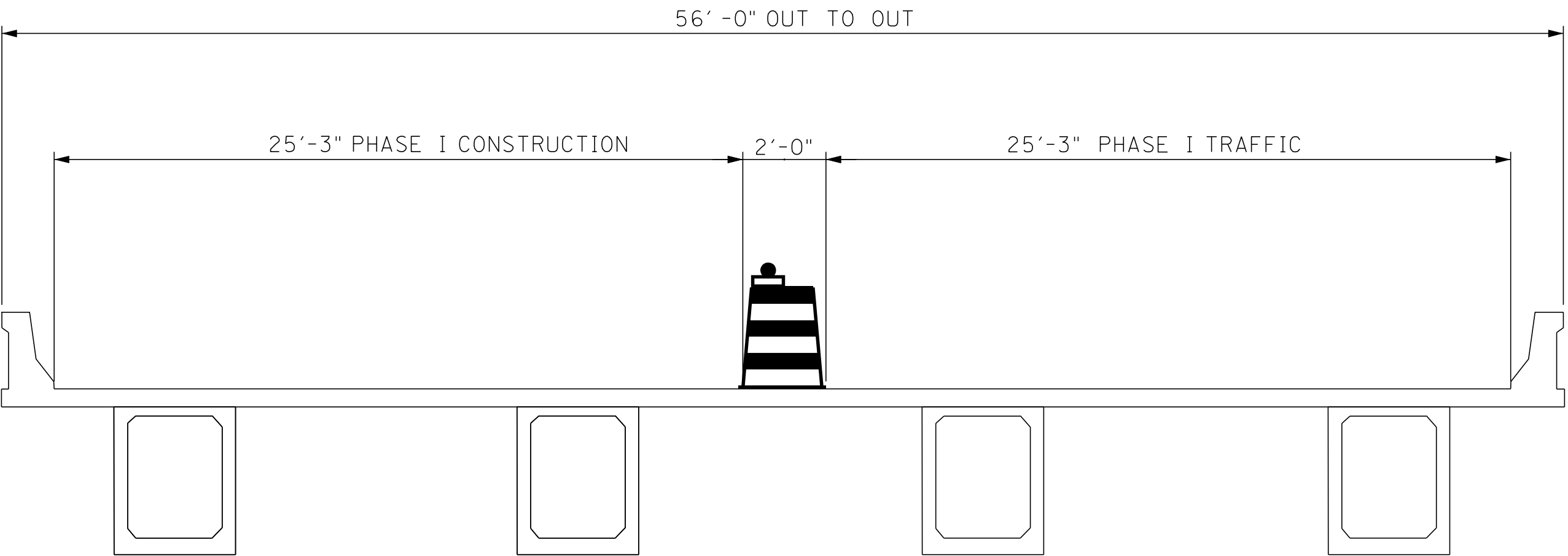
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT DETAILS
AT APPROACHES
PAVEMENTS ENDS
60-SR6-13.61 LT.
OVER ZION ROAD
MAURY COUNTY
2025



PROJECT NO.		YEAR	SHEET NO.
60S006-M3-006		2025	B-6
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1.	12-23-24	B.E.	CHANGED YEAR
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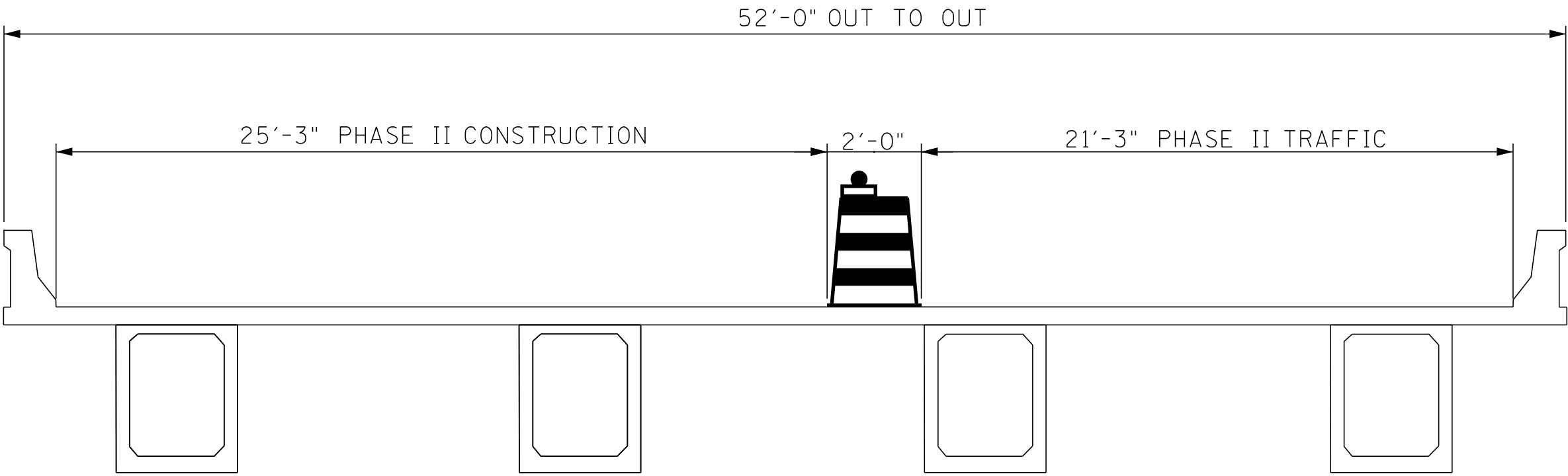


(60-SR6-13.61 LT.)

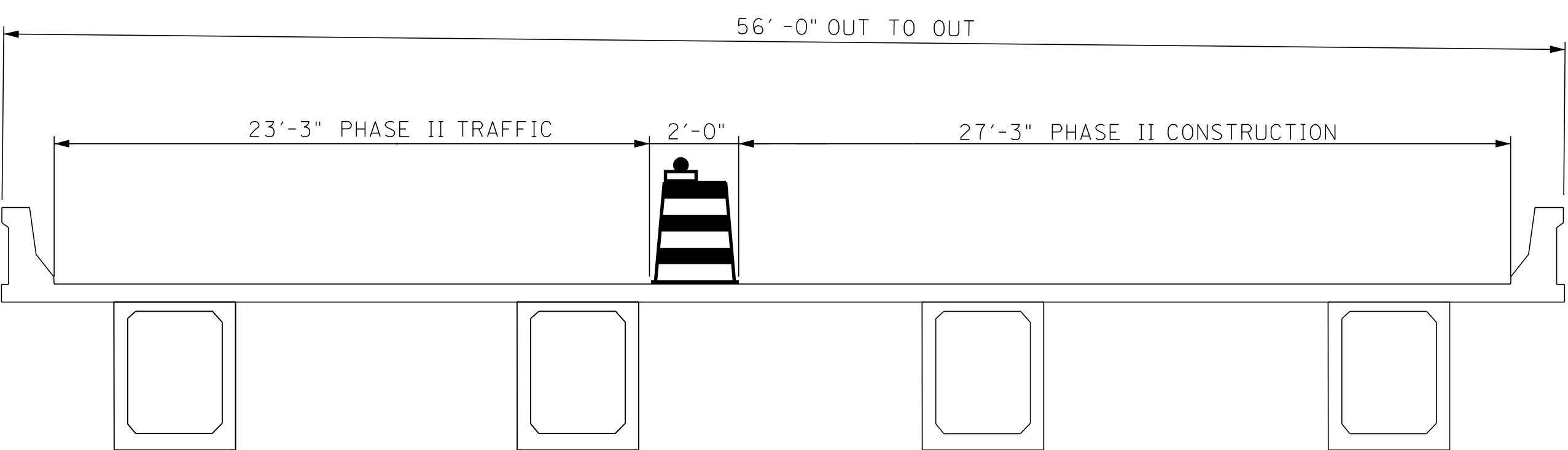


(60-SR6-13.61 RT.)

PHASE I CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



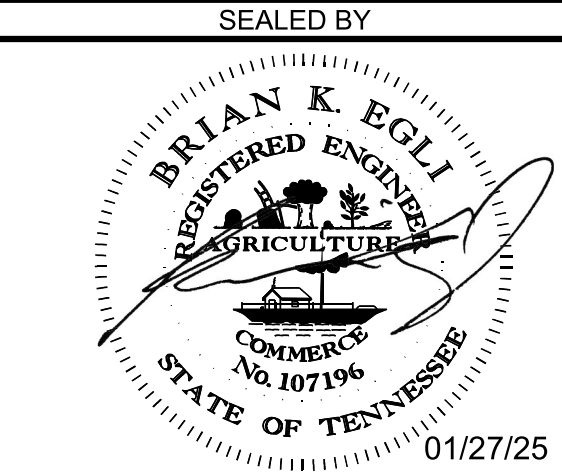
(60-SR6-13.61 LT.)



(60-SR6-13.61 RT.)

PHASE II CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)

PIN NO.:	129517.00	DATE:	/ /
DESIGN BY:		DATE:	12/23
DRAWN BY:	SILESHI ERGICHO	DATE:	12/23
SUPERVISED BY:	KEVIN MARTINKO	DATE:	12/23
CHECKED BY:		DATE:	/ /



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
60-SR6-13.61 RT. & LT.
OVER ZION ROAD
MAURY COUNTY
2025

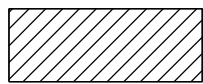
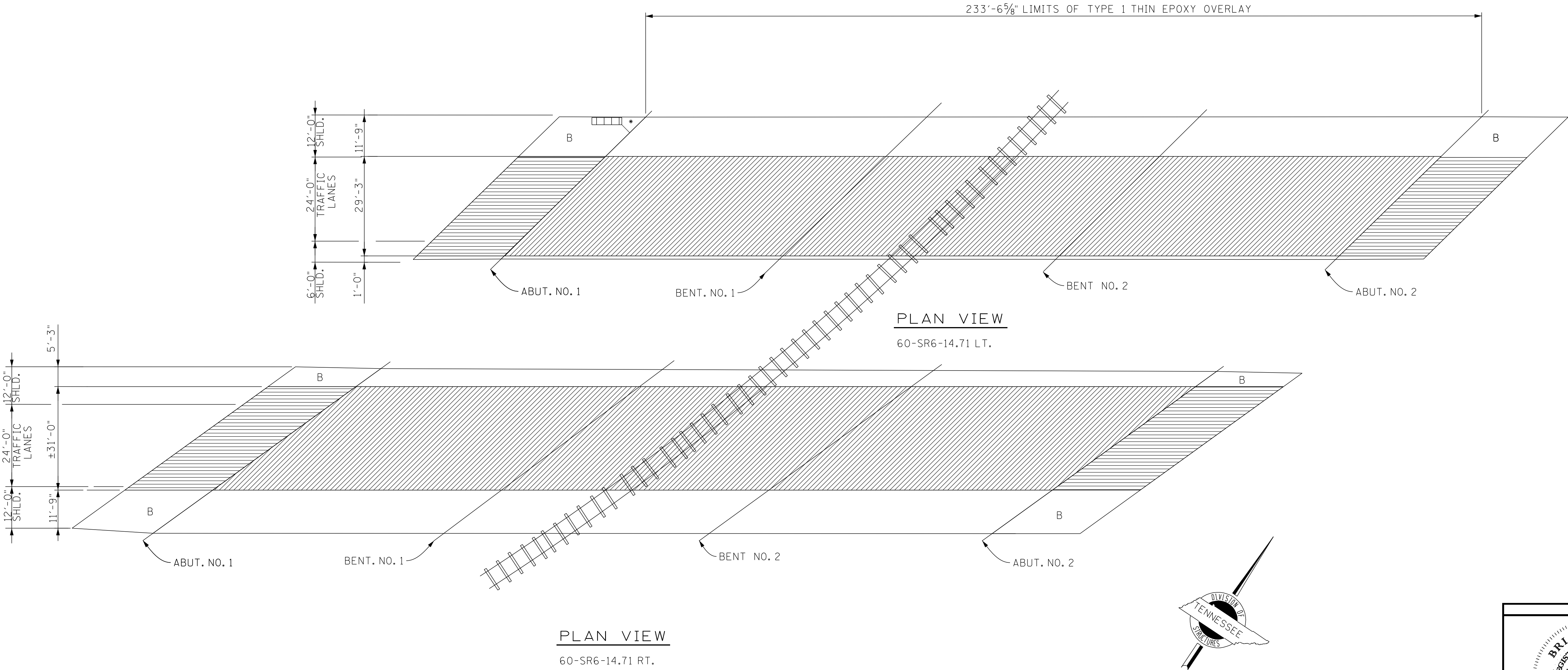


PROJECT NO.		YEAR		SHEET NO.	
60S006-M3-006		2025		B-7	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
1.	12-23-24	B.E.	CHANGED YEAR		
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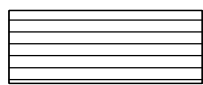
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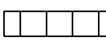
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B

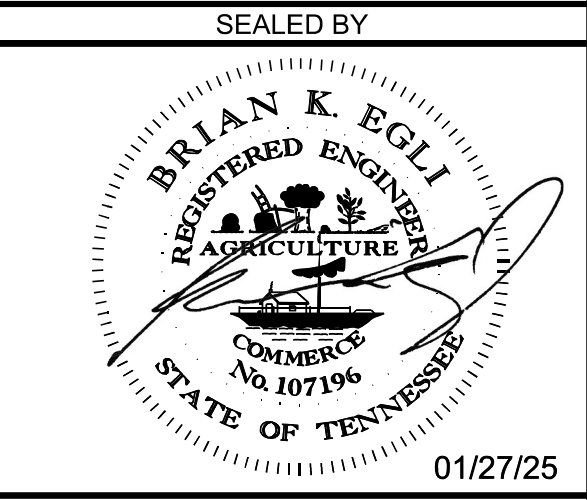
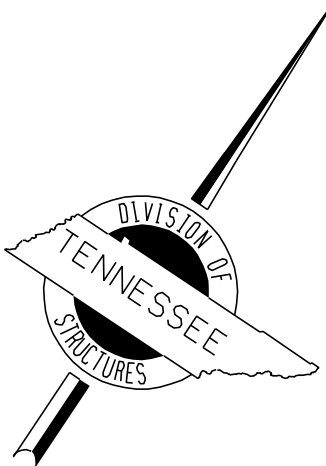
DENOTES: PAVEMENT AT SHOULDER TO BE PAVED EQUIVALENT DEPTH TO THE EXISTING ASPHALT.

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DO NOT PAVE OVER END OF BRIDGE DRAIN



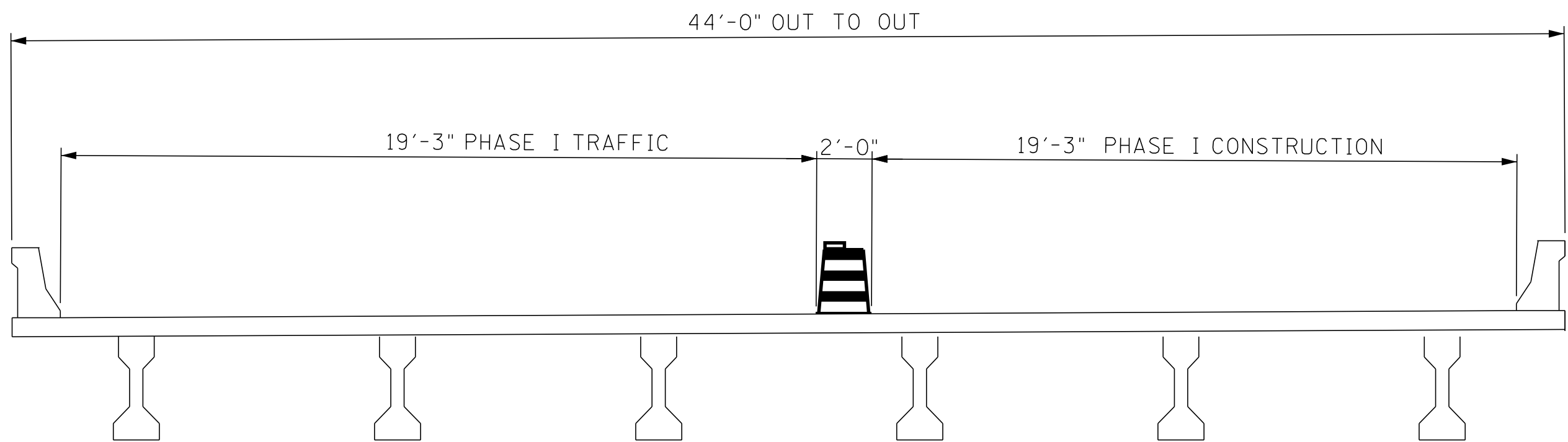
DENOTES: EXISTING BRIDGE END DRAINS



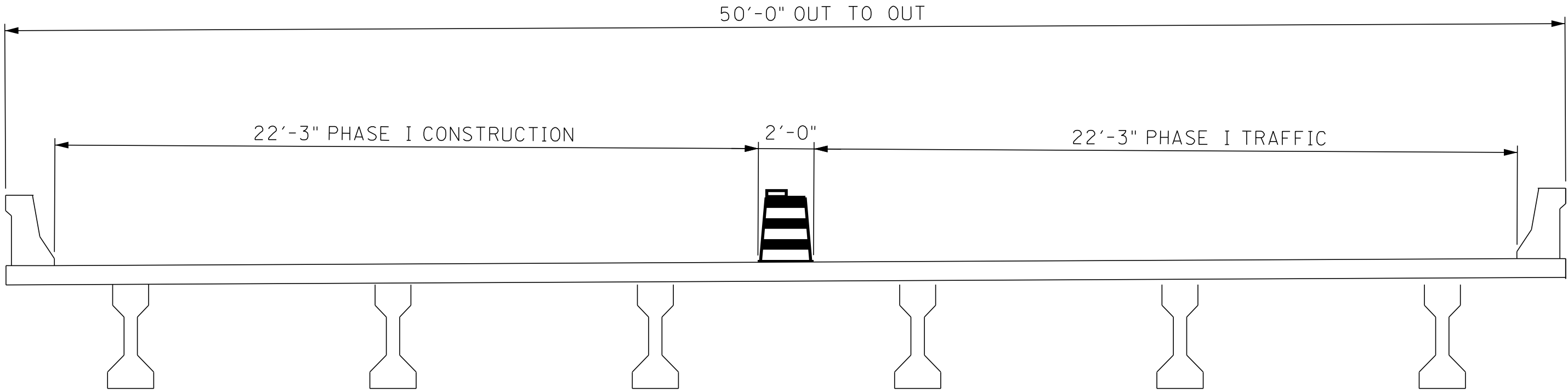
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
APPROXIMATE REPAIRS
LOCATIONS
60-SR6-14.71 RT. & LT. OVER
TENNESSEE SOUTHERN
RAILROAD
MAURY COUNTY
2025

PROJECT NO.		YEAR	SHEET NO.
60S006-M3-006		2025	B-8
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1.	12-23-24	B.E.	CHANGED YEAR
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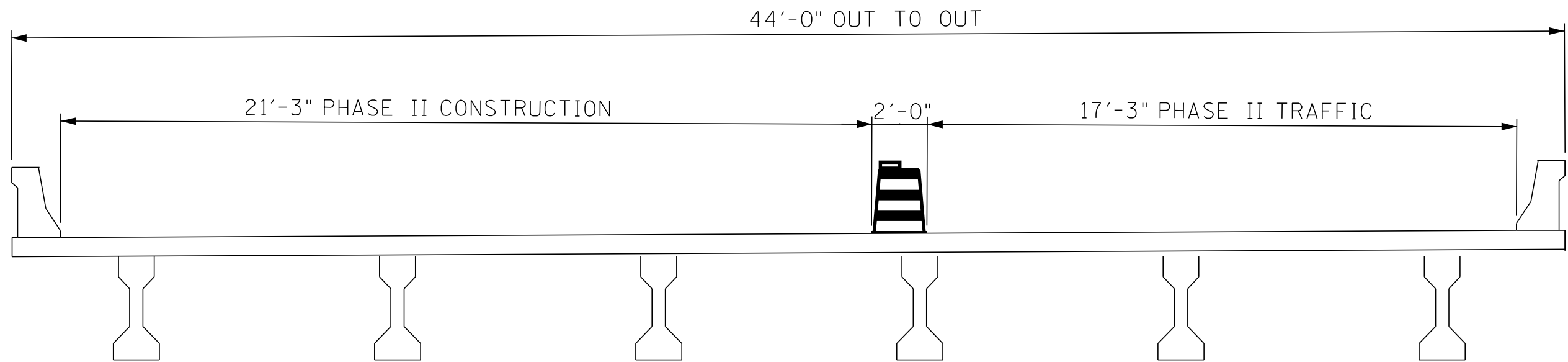
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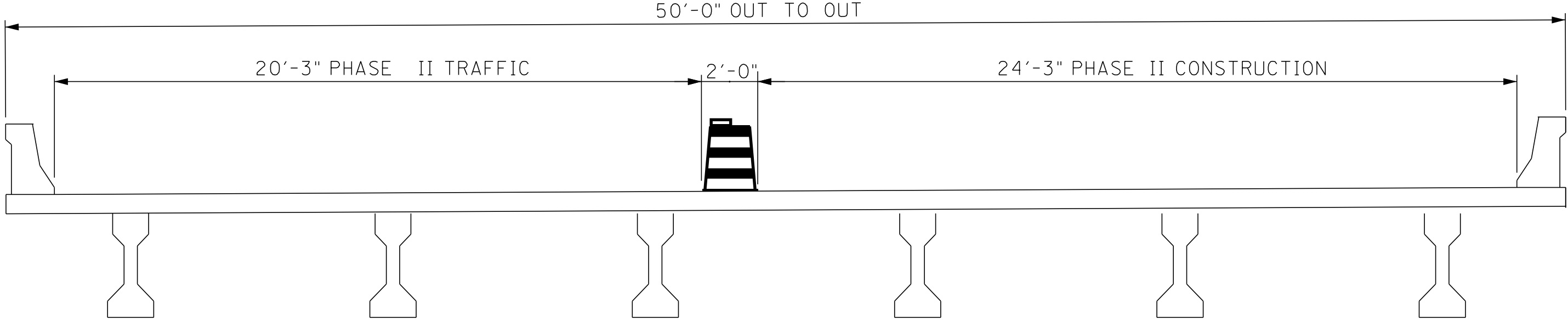
(60-SR6-14.71 RT.)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON THE SURVEY)



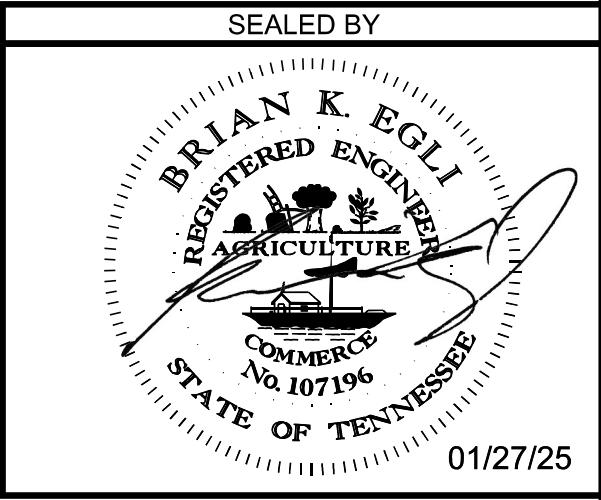
(60-SR6-14.71 LT.)



(60-SR6-14.71 RT.)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

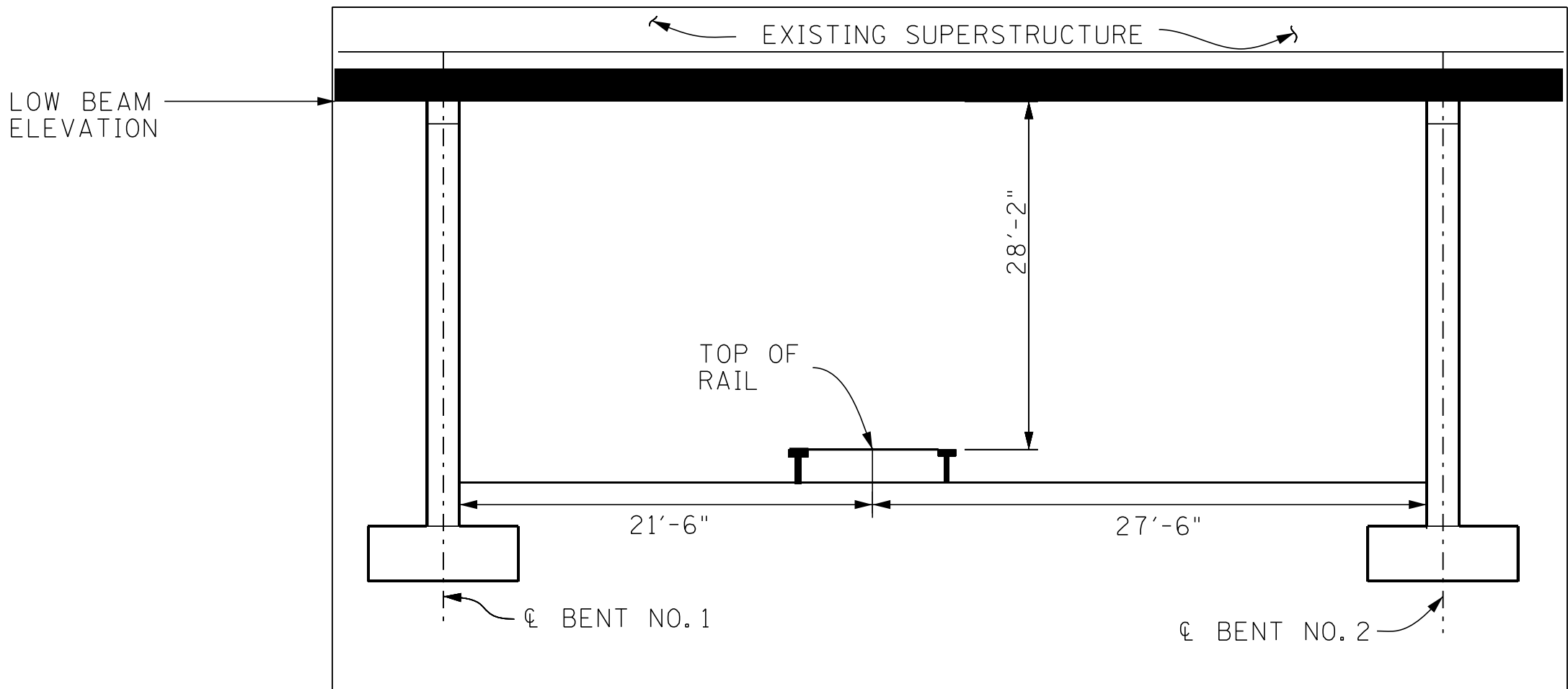
PHASE CONSTRUCTION
60-SR6-14.71 RT.& LT. OVER
TENNESSEE SOUTHERN
RAILROAD
MAURY COUNTY
2025



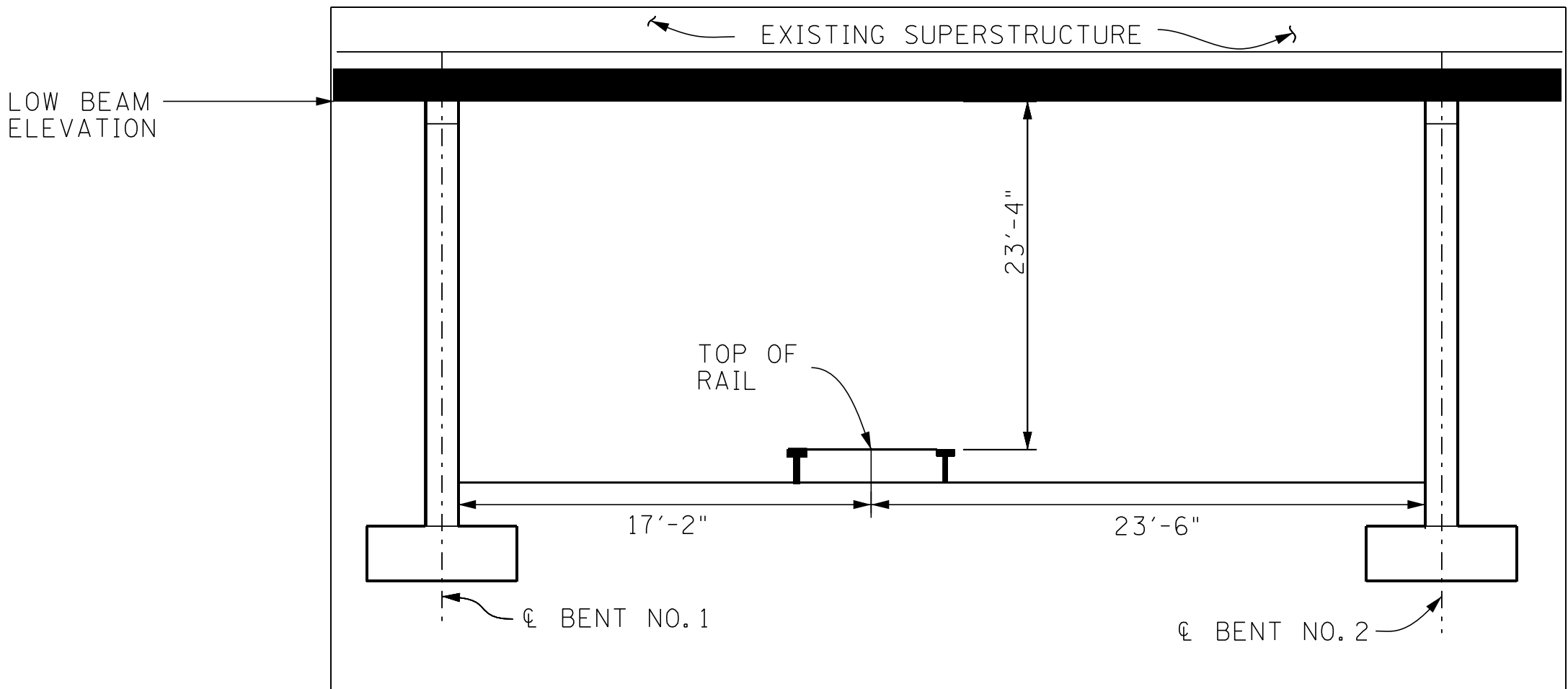
PIN NO.: 129517.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:

DATE: 1/1
DATE: 12/23
DATE: 12/23
DATE: 1/1

PROJECT NO.		YEAR	SHEET NO.
60S006-M3-006		2025	B-9
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1.	12-23-2024	B.E.	CHANGED YEAR
	- -		UPDATED CONTACT RAILROAD ADDRESSES
	- -		
	- -		
	- -		



TENNESSEE SOUTHERN RAILROADR
LEFT LANE BRIDGE.



TENNESSEE SOUTHERN RAILROADR
RIGHT LANE BRIDGE.

SPECIAL RAILROAD NOTES

1.) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY TENNESSEE SOUTHERN RAILROADR ON THIS PROJECT SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER.

2.) THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A DETAILED AND COMPREHENSIVE SCHEDULE, PLAN AND PROCEDURE FOR REVIEW AND APPROVAL BY TENNESSEE SOUTHERN RAILROADR FOR THE FOLLOWING TASKS IN THE RAILROAD'S RIGHT-OF-WAY:

- A) REMOVE EXISTING WEARING SURFACE
- B) BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB)
- C) TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE)

3.) FOR CONCRETE DECK REPAIR OVER THE RAILROAD, DURING DEMOLITION OF THE DECK, A PROTECTION SHIELD SHALL BE ERECTED FROM THE UNDERSIDE OF THE BRIDGE OVER THE TRACK AREA TO CATCH FALLING DEBRIS. THE PROTECTION SHIELD SHALL BE SUPPORTED FROM GIRDERS OR BEAMS. THE PROTECTION SHIELD SHALL BE DESIGNED, WITH SUPPORTING CALCULATIONS, FOR A MINIMUM OF 50 POUNDS PER SQUARE FOOT PLUS THE WEIGHT OF THE EQUIPMENT, DEBRIS, PERSONNEL, AND OTHER LOADS TO BE CARRIED.

LARGE PIECES OF THE DECK SHALL NOT BE ALLOWED TO FALL ON THE PROTECTION SHIELD

A BALLAST PROTECTION SYSTEM CONSISTING OF GEOFABRIC OR CANVAS SHALL BE PLACED OVER THE TRACK STRUCTURE TO KEEP THE BALLAST CLEAN. THE SYSTEM SHALL EXTEND ALONG THE TRACK STRUCTURE FOR A MINIMUM OF 25'-0" BEYOND THE LIMITS OF THE DEMOLITION WORK, OR FARTHER IF REQUIRED BY TSRR'S CONSTRUCTION ENGINEERING AND INSPECTION REPRESENTATIVE.

THE CONTRACTOR SHALL SUBMIT DETAILED PLANS TO TENNESSEE SOUTHERN RAILROADR RAILROAD , WITH SUPPORTING CALCULATIONS, OF THE PROTECTION SHIELD AND BALLAST PROTECTION SYSTEMS FOR APPROVAL PRIOR TO THE START OF DEMOLITION.

COST OF REMOVING AND DISPOSING OF DEBRIS, COMPLETE AND FULL PROTECTION OF THE CONTRACTOR WORK AREA FOR DEMOLITION SHALL BE INCLUDED IN ITEMS BID ON.

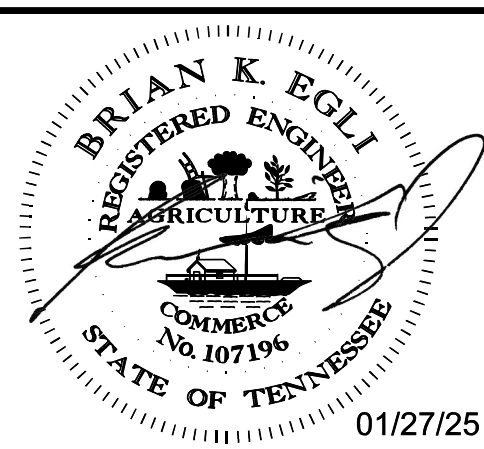
4.) NO TEMPORARY FALSEWORK IF APPLICABLE WILL BE ALLOWED THAT INFRINGES ON EXISTING HORIZONTAL AND VERTICAL CLEARANCES (SEE THIS SHEET) WITHOUT PRIOR APPROVAL OF TENNESSEE SOUTHERN RAILROAD. IT IS ANTICIPATED THAT FULL TIME FLAGGING WILL ONLY BE REQUIRED FOR FULL DEPTH CONCRETE DECK REPAIRS, AND ANY OTHER FLAGGING SERVICES REQUIREMENTS FOR THE BRIDGE DECK SEAL REPAIR CONSTRUCTION WILL BE AT THE DISCRETION OF THE RAILROAD.

5.) SEE RAILROAD AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C(R) (RAILROAD SPECIAL PROVISIONS) AND TENNESSEE SOUTHERN RAILROAD PUBLIC PROJECTS MANUAL CURRENT EDITION TO SET UP FLAGGING SERVICES, TO SUBMIT FOR INSURANCE REQUIRED FOR WORK ON HIGHWAY BRIDGE OVER THE RAILROAD, AND FOR INFORMATION ON ALL OTHER RAILROAD RULES AND SPECIFICATIONS THAT APPLY TO THIS PROJECT.

RAILROAD CONTACT ADDRESSES

TERRY J. FRANK
DIRECTOR OF ENGINEERING
XORAIL, INC. (REPERSENTING TENNESSEE SOUTHERN RR)
5011 GATE PARKWAY : BUILDING 100, SUITE 400
JACKSONVILLE, FL 32256
OFFICE: (904) 596-1213
FAX: (904) 443-0089
:EMAIL: tfrank@wabtec.com

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION



VERTICAL CLEARANCES
AND RAILROAD NOTES
60-SR6-14.71 RT. & LT. OVER
TENNESSEE SOUTHERN
RAILROAD
MAURY COUNTY
2025

PIN NO.: 129517.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:

DATE: / /
DATE: 12/23
DATE: 12/23
DATE: / /

CONST. NO. 60092-3417-04

PROJECT NO.	YEAR	SHEET NO.
F-7 (5)	1987	
REVISIONS		
NO.	DATE	BY

LIST OF DRAWINGS

BRIDGE DRAWINGS	DRAWING NO.	LAST REV. DATE
LAYOUT	M-144-14	
ESTIMATED QUANTITIES AND GENERAL NOTES	M-144-15	
FOUNDATION DATA	M-144-16	
SUPERSTRUCTURE	M-144-17	
SUPERSTRUCTURE DETAILS	M-144-18	
PRESTRESSED BOX BEAM DETAILS	M-144-19	
ABUTMENT NO. 1 & 2 (EAST BOUND LANES)	M-144-20	
ABUTMENT NO. 1 & 2 DETAILS (E.B.L.)	M-144-21	
ABUTMENT NO. 1 & 2 DETAILS (E.B.L.)	M-144-22	
ABUTMENT NO. 1 & 2 (WEST BOUND LANES)	M-144-23	
ABUTMENT NO. 1 & 2 DETAILS (W.B.L.)	M-144-24	
ABUTMENT NO. 1 & 2 DETAILS (W.B.L.)	M-144-25	
BILL OF STEEL	M-144-26	
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	M-144-27	2-09-87
STD. PILE DETAILS	M-174-150	12-19-86
TENN. STD. PRECAST PRESTRESSED BRIDGE DECK PANELS	M-164-24	LAST
STANDARD DRAWINGS	DRAWING NO.	REV. DATE
STD. REINF. BAR SUPPORT DETAILS	K-60-14	8/27/76
TENN. STD. PRECAST PRESTRESSED BRIDGE DECK PANELS	M-164-25	2/04/86
STD. REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	K-86-144	7/17/81
BRIDGE RAILING, CONCRETE PARAPET	M-28-1	10/22/86
GRADING DESIGN & SLOPE PROTECTION	RD-SA-1	1/31/84

LIST OF SPECIAL PROVISIONS

SPECIAL PROV. NO.	REGARDING	LAST REV. DATE
604P	PRECAST PRESTRESSED DECK PANELS	11-11-85
105A	APPROVAL OF SHOP DRAWINGS	3/25/85
604C	STRUCTURAL CONCRETE	9/22/86
604	CONCRETE STRUCTURES	11/03/86
907A	REGARDING EPOXY COATED REINFORCING STEEL	3/25/85
604R	REGARDING THE RIDEABILITY OF BRIDGE DECKS AND ROADWAY APPROACHES	2/09/87

ESTIMATED 2,003 A.D.T. =

50'-0" ROADWAY WITH CONCRETE PARAPET (WEST BOUND)
54'-0" ROADWAY WITH CONCRETE PARAPET (EAST BOUND)

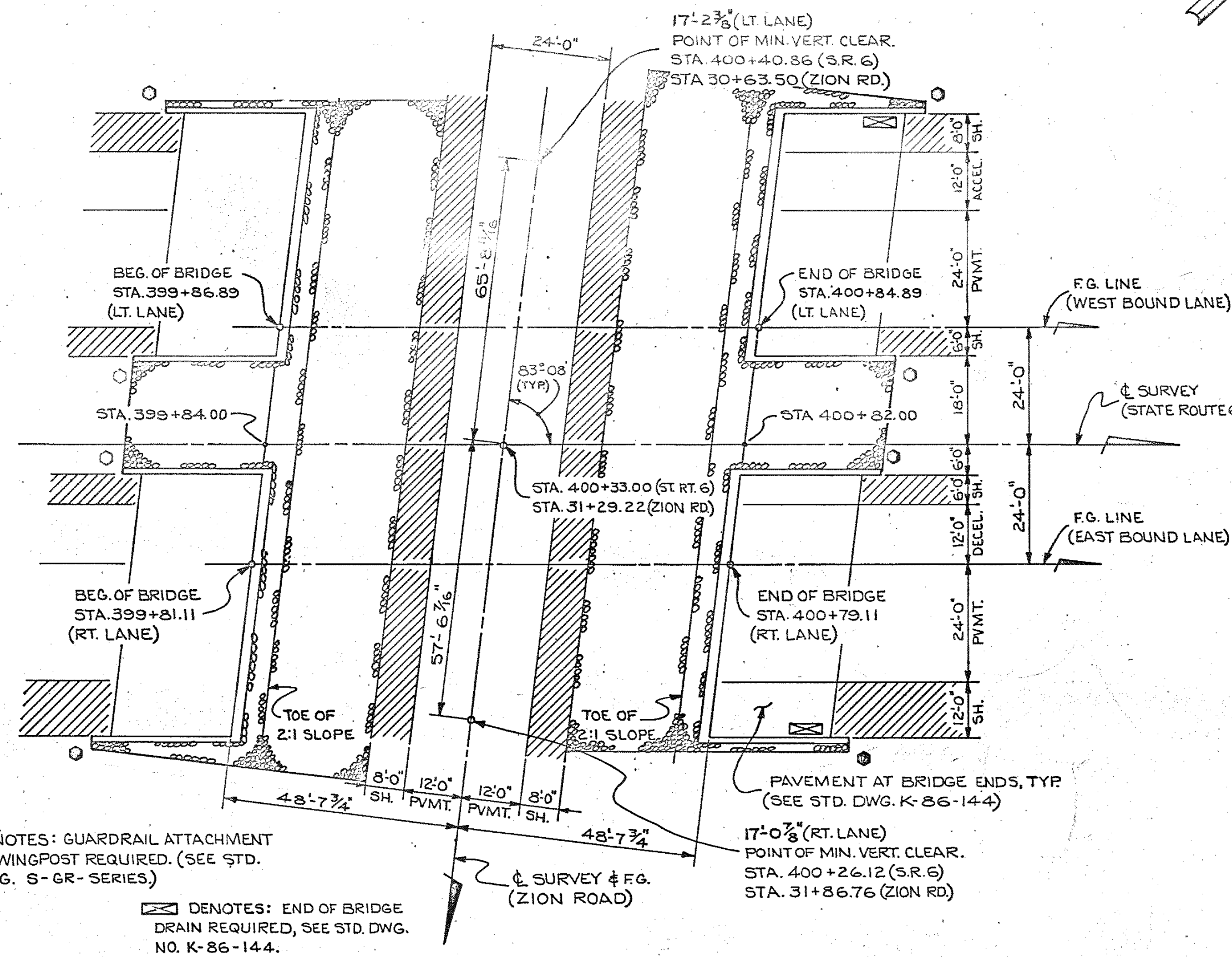
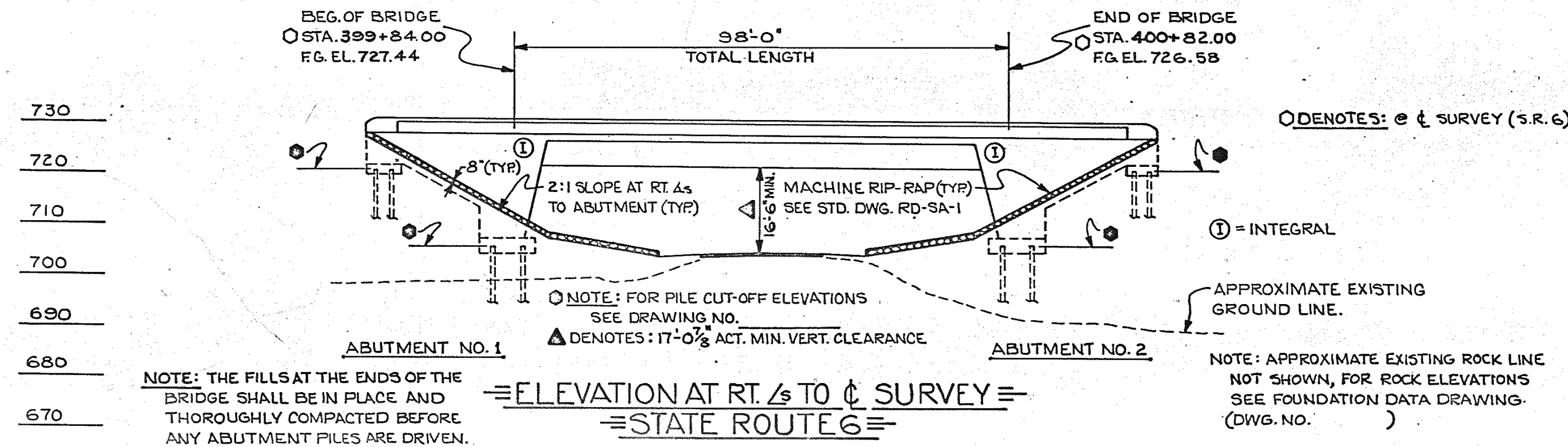
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

LAYOUT OF BRIDGE
STATE ROUTE NO. 6 OVER
ZION ROAD
STATION 400+33.00
MAURY COUNTY
-1987-

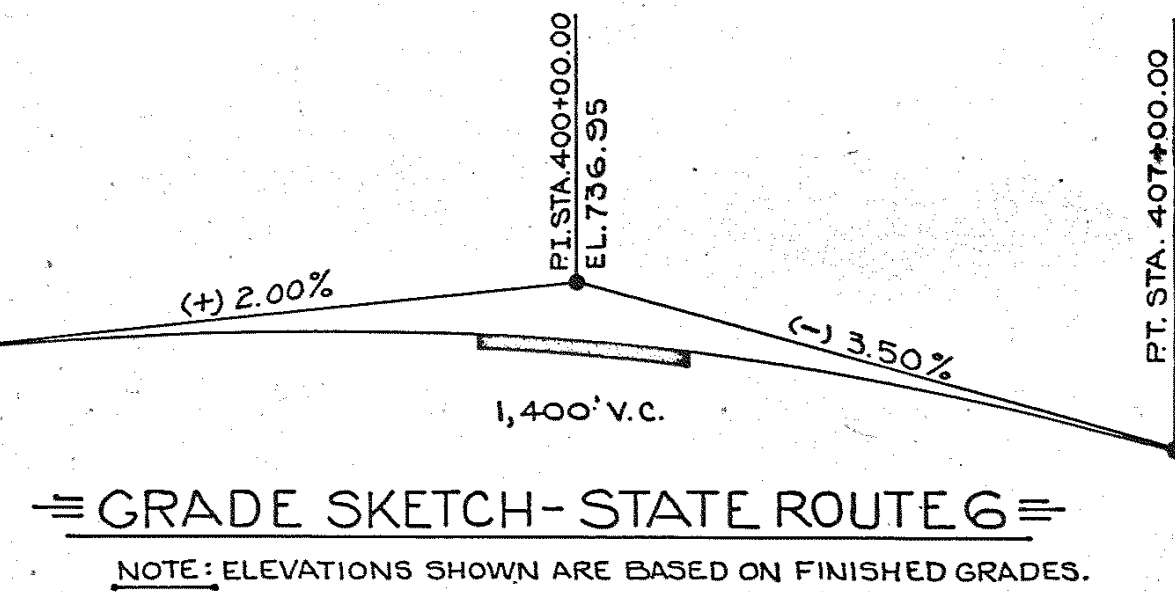
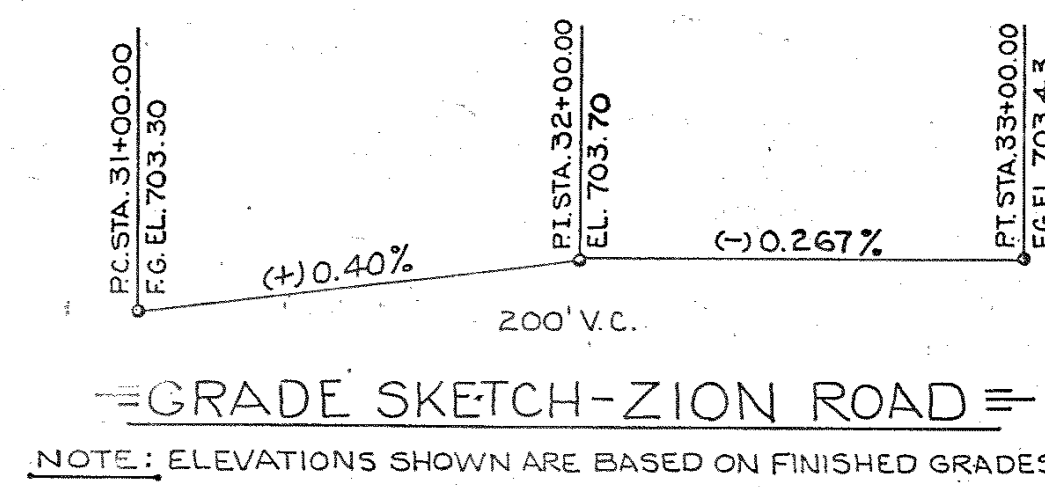
CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES
APPROVED *Lewis Evans*
DIRECTOR OF HIGHWAYS

M-144-14

CLASS 'A' GRADING 'D' 3,130 C.Y.



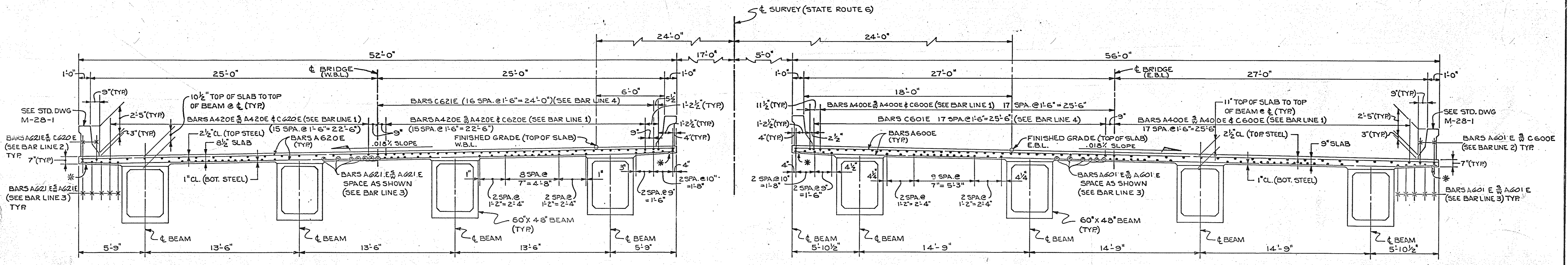
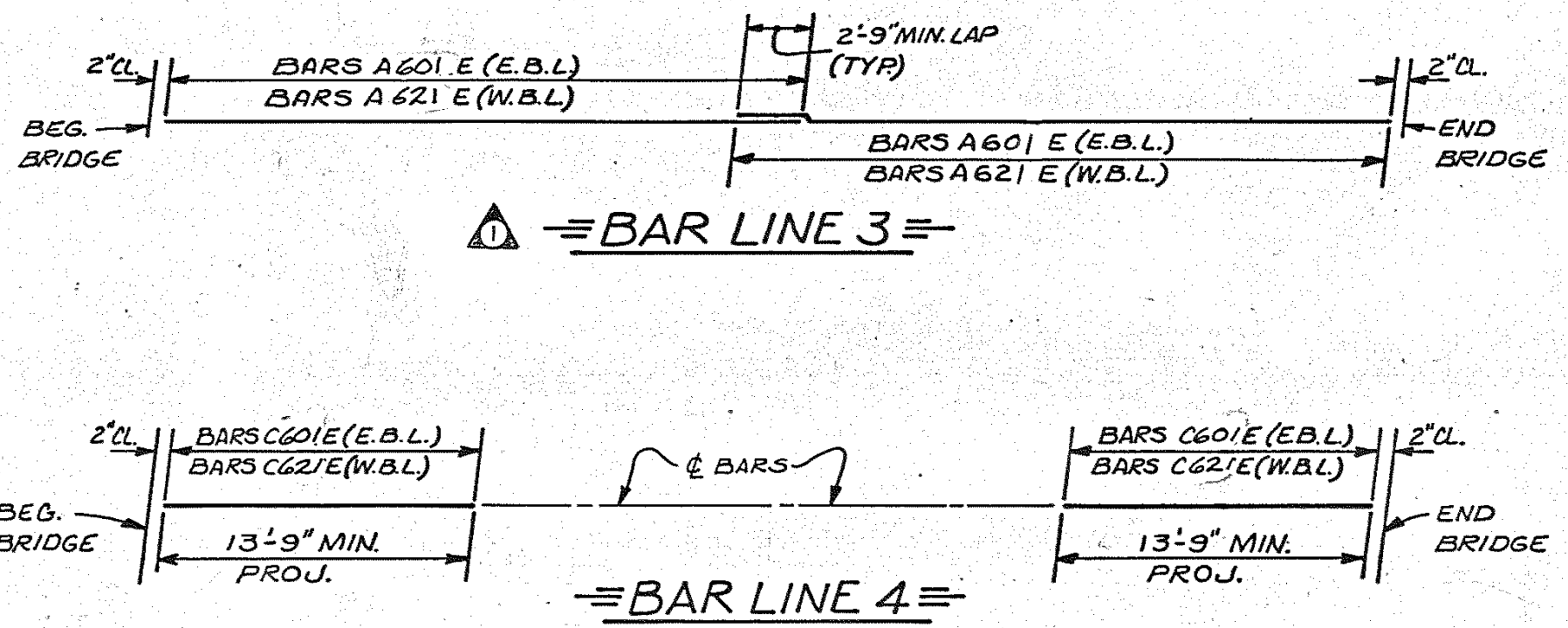
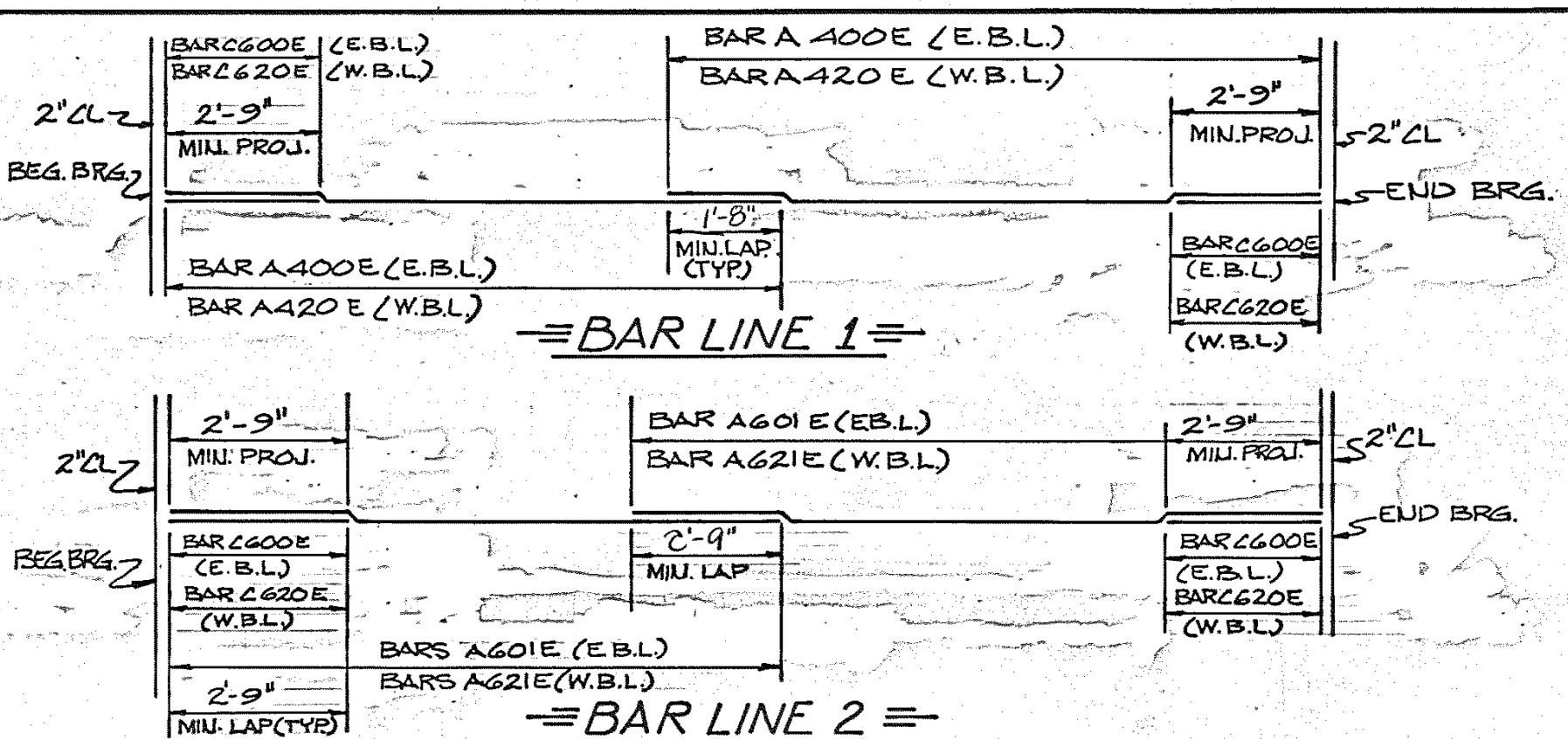
BRIDGE SCREED M-144-18A
APPROACH ELEVATIONS M-144-18B



DESIGNED BY STEVE NICHOLS
DRAWN BY S. KEMP
SUPERVISED BY
CHECKED BY W. MACKIE

DATE 4/5/83
DATE 4/13/83
DATE
DATE 6/27/83

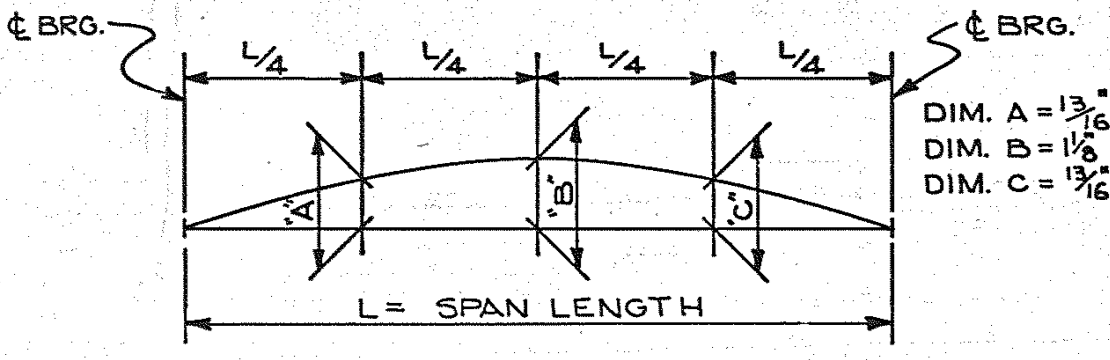
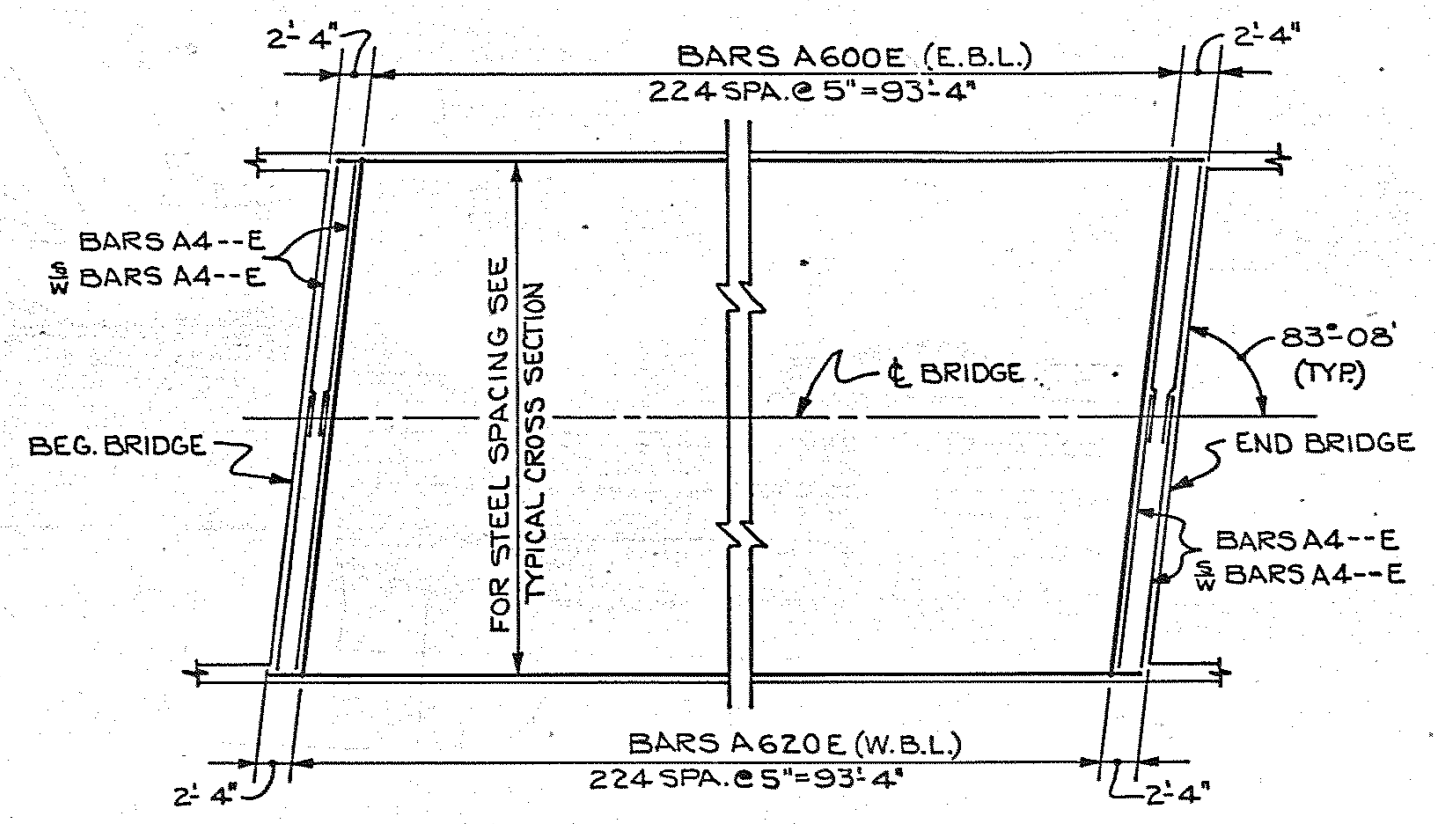
PROJECT NO.		YEAR	SHEET NO.
		1987	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	9-9-87	WWM	BAR LINE 3



TYPICAL CROSS SECTION
(LOOKING FORWARD ON SURVEY)

WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED.



THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

ESTIMATED QUANTITIES

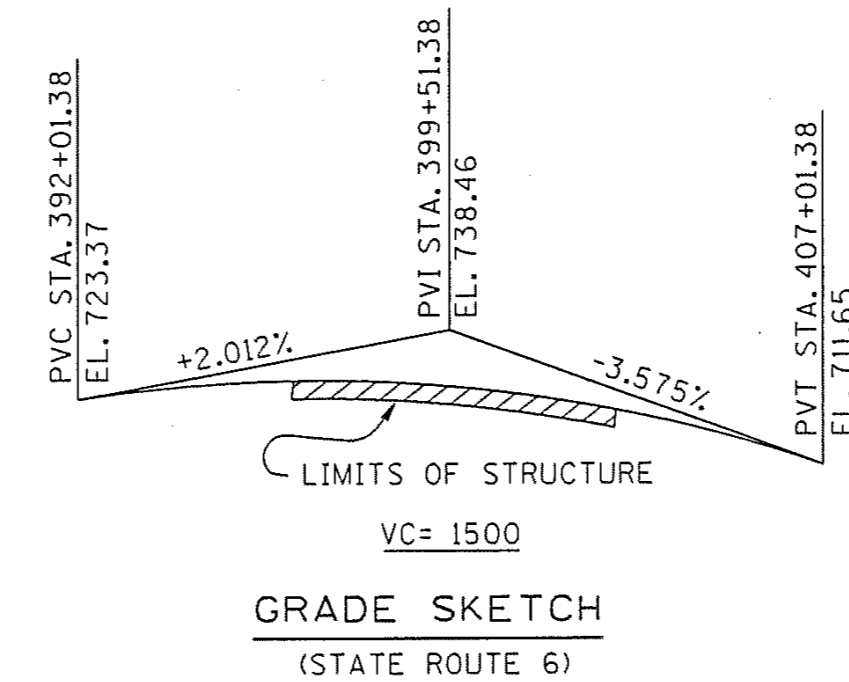
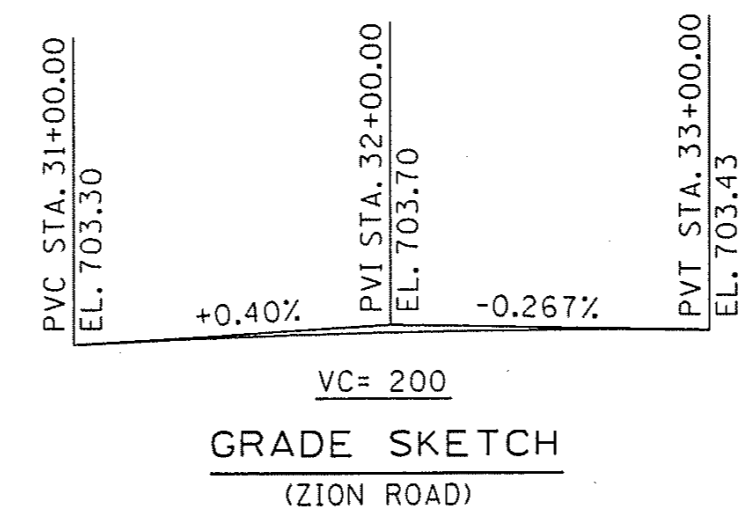
ITEM	CONCRETE CLASS "A" C.Y.	REINFORCING STEEL LBS.	EPOXY COATED REINF. STEEL L.B.S.
SUPERSTRUCTURE (E.B.L.)	247	1,554	58,925
SUPERSTRUCTURE (W.B.L.)	223	1,492	49,325

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
SUPERSTRUCTURE
STATE ROUTE NO. 6 OVER
ZION ROAD
STATION 400+33.00
MAURY COUNTY
-1987-

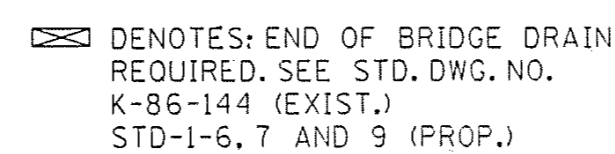
DESIGNED BY STEVE NICHOLS
DRAWN BY S. KEMP
SUPERVISED BY
CHECKED BY W. MACKIE

DATE 4/5/83
DATE 4/5/83
DATE
DATE 4/27/83

CORRECT Edward R. Wasserman
ENGINEER OF STRUCTURES
APPROVED Lewis Evans
DIRECTOR OF HIGHWAYS

[illegible]

ELEVATION
(SCALE: 1"= 20'-0")
(SHOWING WESTBOUND LANE)



DESIGNED BY C. DIETERS DATE 02-99
DRAWN BY J. CANTRELL DATE 02-99
SUPERVISED BY W.H.P./ J.W.F. DATE 02-99
CHECKED BY C. DIETERS DATE 04-99

LIST OF DRAWINGS	DWG. NO.	REV. DATE
LAYOUT OF BRIDGE.....	M-379-1	8-12-99
GENERAL NOTES & ESTIMATED		
QUANTITIES.....	M-379-2	
FOUNDATION DATA.....	M-379-3	
FOUNDATION DATA.....	M-379-4	
SUPERSTRUCTURE.....	M-379-5	
SUPERSTRUCTURE DETAILS.....	M-379-6	
PRESTRESSED BOX BEAM DETAILS.....	M-379-7	
ABUTMENT NO. 1.....	M-379-8	
ABUTMENT NO. 2.....	M-379-9	
ABUTMENT NO. 1 & 2 DETAILS.....	M-379-10	
ABUTMENT NO. 1 & 2 DETAILS.....	M-379-11	
FINAL FOUNDATION DATA.....	M-379-12	
BILL OF STEEL.....	M-379-13	
BILL OF STEEL.....	M-379-14	

<u>LIST OF STANDARD DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
BRIDGE RAILING CONCRETE PARAPET.....	STD-1-1	4-28-97
SLIDER PLATES AND DECK DRAINS.....	STD-1-2	9-11-95
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS.....	STD-1-5	4-28-97
BRIDGE END DRAIN DETAILS 2'X8'-7" e 4'X8'-7" WITH PAVEMENT AT BRIDGE ENDS.....	STD-1-6	4-28-97
BRIDGE END DRAIN DETAILS 2'X8'-7" e 4'X8'-7" WITH PAVEMENT AT BRIDGE ENDS.....	STD-1-7	4-28-97
BRIDGE END DRAIN DETAILS 2'X8'-7" e 4'X8'-7" WITH PAVEMENT AT BRIDGE ENDS.....	STD-1-9	5-01-95
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS.....	STD-4-1	4-28-97
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA.....	STD-4-2	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS.....	STD-4-3	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS.....	STD-4-4	6-10-96
STD. PILE DETAILS.....	STD-5-1	10-25-93
STD. PILE DETAILS.....	STD-5-2	10-26-92
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB.....	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS.....	STD-10-1	5-11-92
STD. DETAILS FOR PRESTRESSED BOX BEAMS.....	STD-14-3	
SLOPE PROTECTION.....	RD-SA-1	5-27-98

<u>LIST OF</u> <u>SPECIAL PROV.</u>	<u>PROV. NO.</u>	<u>LAST</u> <u>REV. DATE</u>
APPROVAL OF SHOP DRAWINGS.....	105A 12-15-97

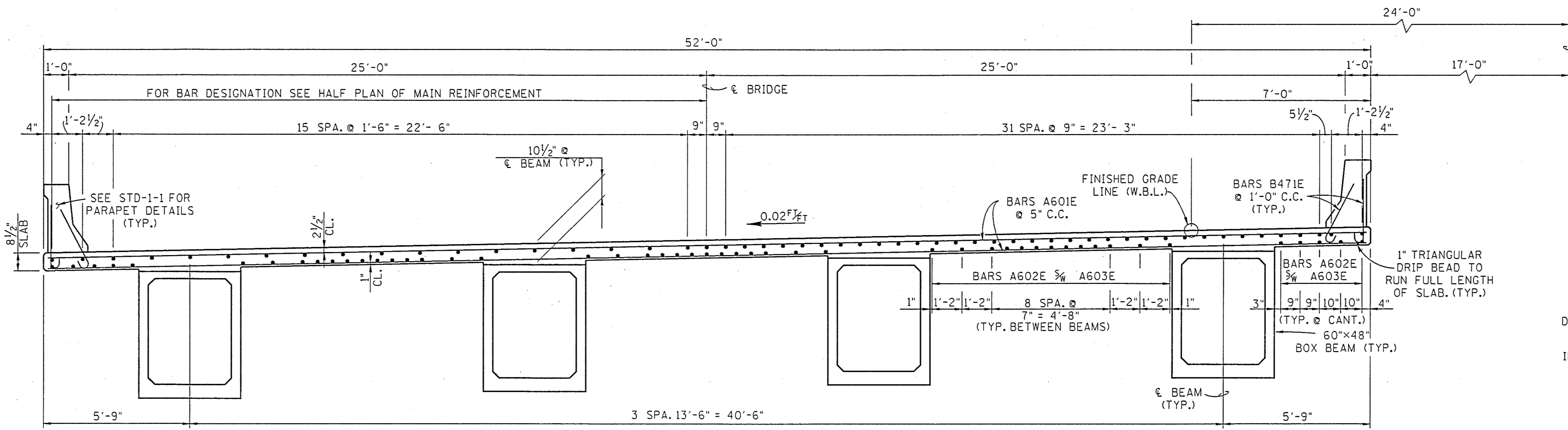
2006 ADT = 16,130
50'-0" ROADWAY W/STD-1-1 PARAPET (WESTBOUND LANE)
DESIGN SPEED = 70 MPH

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 1
LAYOUT OF BRIDGE
WESTBOUND LANE
STATE ROUTE 6
OVER
ZION ROAD
BRIDGE I.D. NO. 60SR0060077
STATION 399+84.12
MAURY COUNTY
1999

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

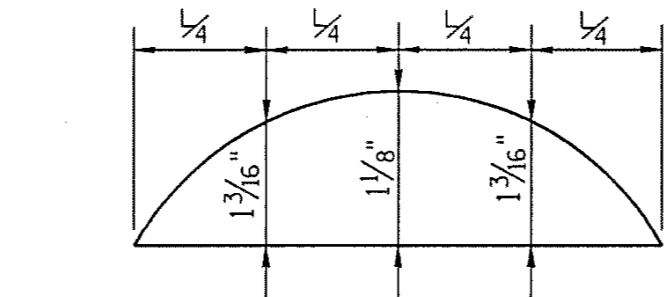
4-379-01



HALF SECTION NEAR @ MID-SPAN

HALF SECTION NEAR @ SUPPORT

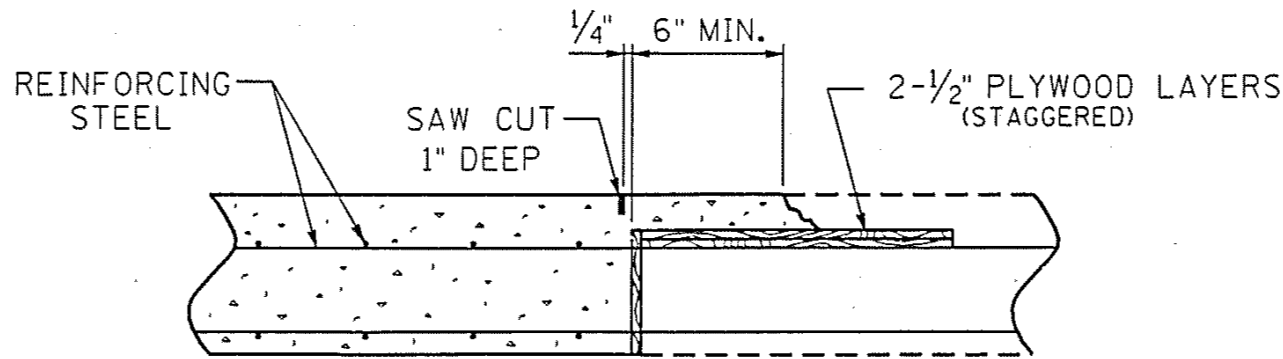
TYPICAL CROSS SECTION
(LOOKING FORWARD ON SURVEY)



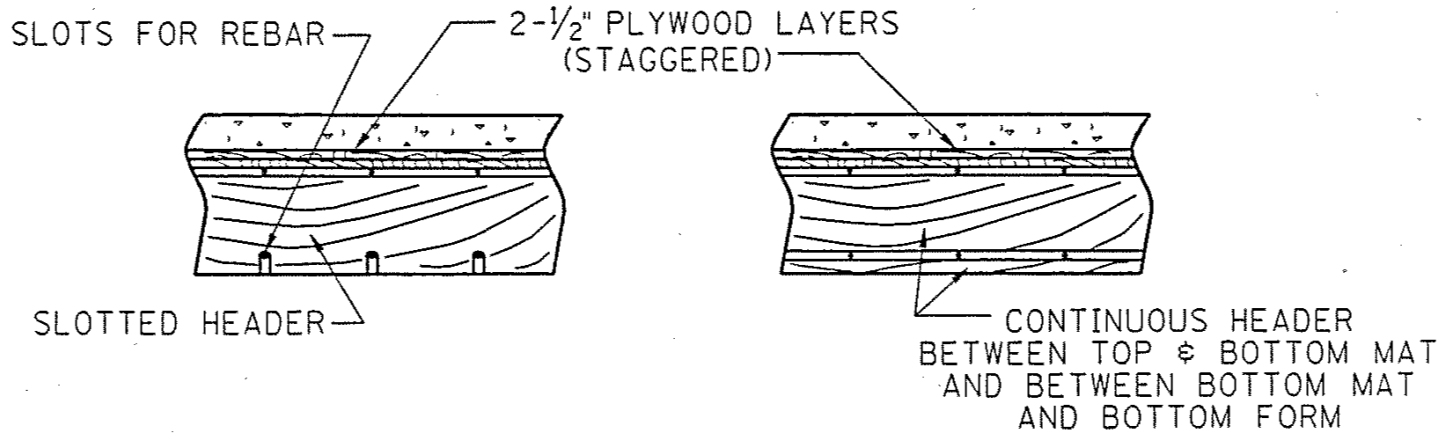
DEAD LOAD
CORRECTION CURVE

DEAD LOAD CORRECTION CURVE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:
1. NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM A SUPPORT.
2. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN BELOW.



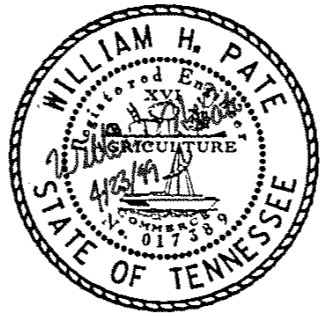
SLAB CONSTRUCTION JOINT DETAIL



ALTERNATE "A" ALTERNATE "B"
ALTERNATE HEADER DETAILS

ESTIMATED QUANTITIES

CLASS "D" CONCRETE (BRIDGE DECK) C.Y.	EPOXY-COATED REINFORCING STEEL (BRIDGES) LB.
137	47,119



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 1
SUPERSTRUCTURE
WESTBOUND LANE
STATE ROUTE 6
OVER
ZION ROAD
STATION 399+84.12
MAURY COUNTY
1999

CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES

GENERAL NOTES:

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.
NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

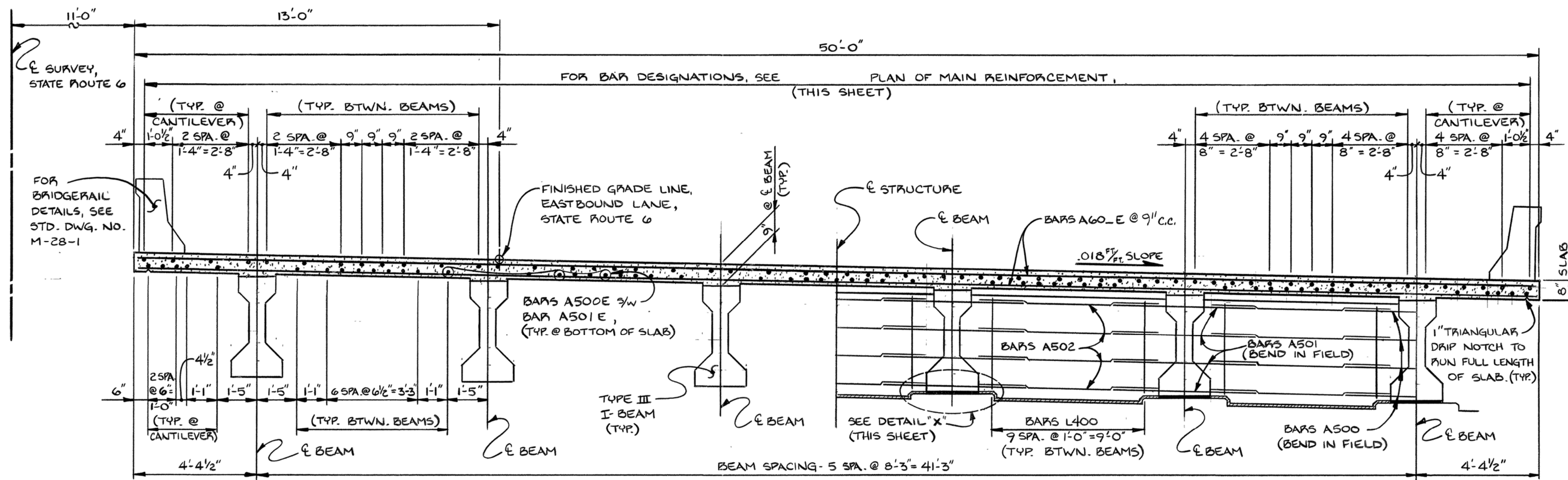
NOTE: WHEN POURING SLAB PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.
ALSO SEE STANDARD DWG. NO. STD-1-1.

DESIGNED BY SABRINA DIETERS DATE 02-99
DRAWN BY G. W. KORNICKSKI DATE 02-99
SUPERVISED BY J. W. F. / W. H. P. DATE 02-99
CHECKED BY S. DIETERS DATE 04-99

CONST. NO. 60092-3219-14

PROJECT NO.	YEAR	SHEET NO.
F-6(15)	1986	

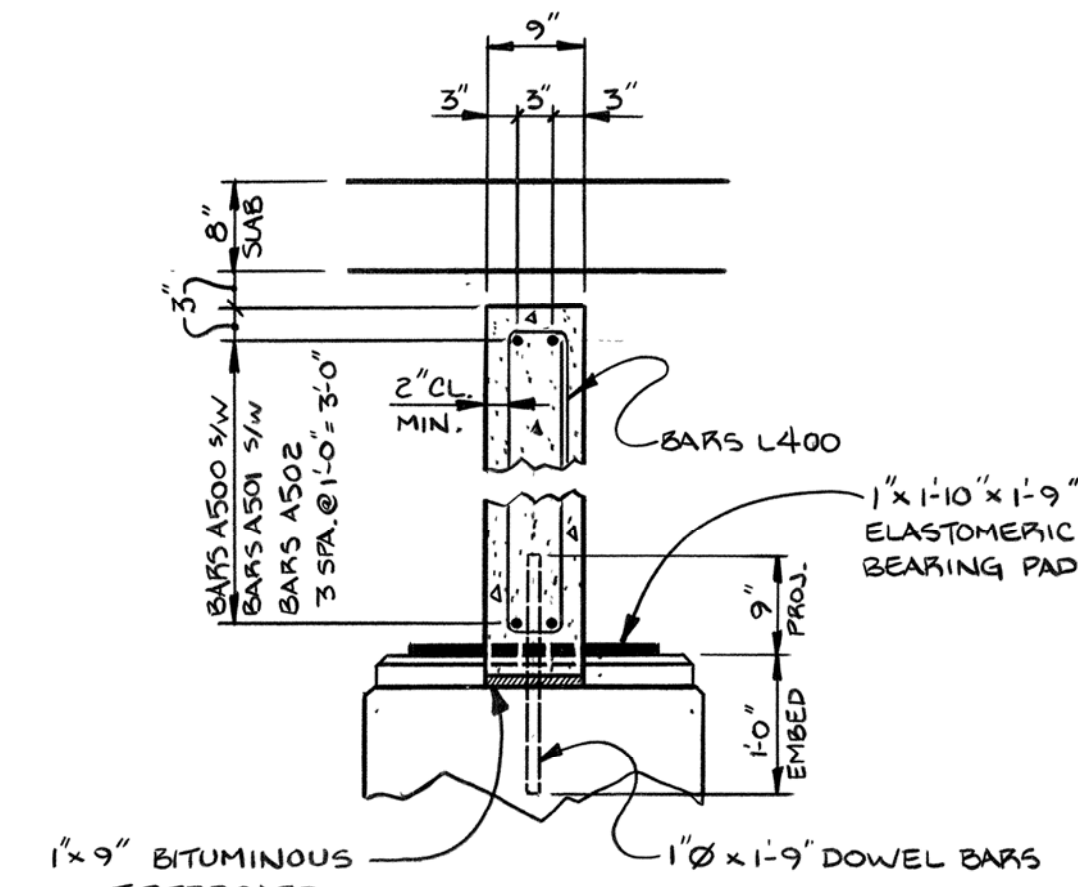
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



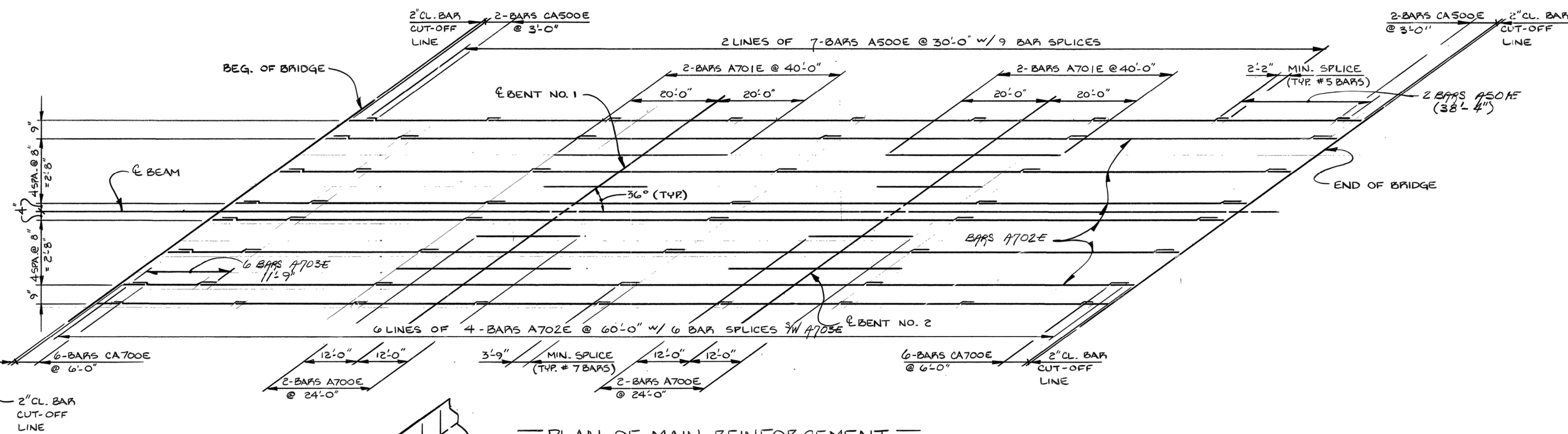
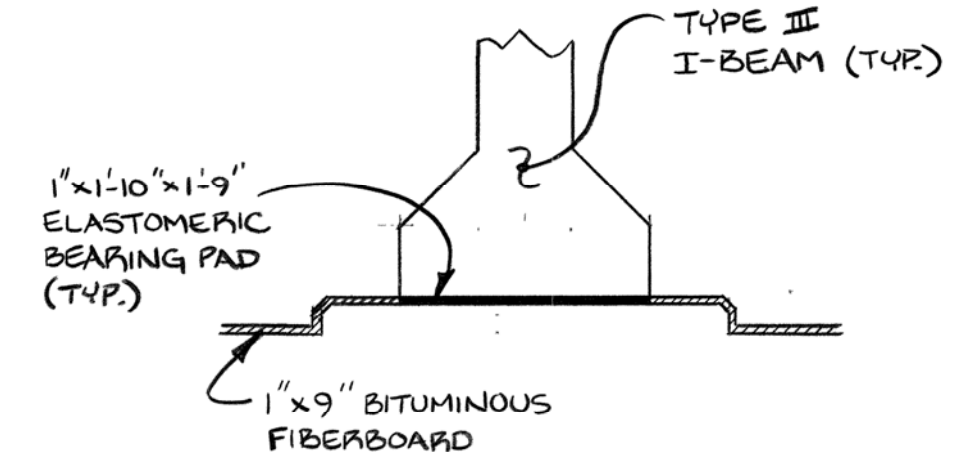
HALF-SECTION @ MID-SPAN

HALF-SECTION @ SUPPORT

TYPICAL CROSS-SECTION



SPECIAL NOTE FOR DOWEL BARS AT BENTS: TOP OF DOWELS TO BE COVERED WITH 1/2" OF COMPRESSIBLE MATERIAL AND THE 9" PROJECTION WRAPPED WITH TWO LAYERS OF WATERPROOF PAPER.

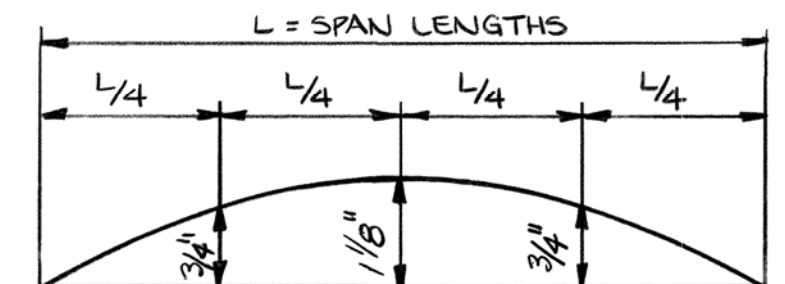


PLANS NOTE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION EXCEPT NO JOINT MAY BE LOCATED CLOSER THAN 1/5 SPAN LENGTH FROM AN INTERIOR SUPPORT. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

NOTE: ALL GIRDETS TO BE SUPPORTED DURING CONSTRUCTION OF SLAB TO PREVENT ROTATION.



DEAD LOAD CORRECTION CURVE

THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE.

ESTIMATED QUANTITIES

ITEM	CLASS "A" CONCRETE (BRIDGES) C.Y.	CLASS "A" CONCRETE (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LBS.	REINFORCING STEEL LBS.
SUPERSTRUCTURE	58	294	88,848	5,068

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
SUPERSTRUCTURE
STATE ROUTE 6 OVER
SEABOARD SYSTEM SPUR NO. 1
STATION 459+32.97
MAURY COUNTY
1986

CORRECT *Chellon L. Lora*
ENGINEER OF STRUCTURES
APPROVED *Shirley Evans*
DIRECTOR OF HIGHWAYS

M-171-6

DESIGNED BY KEN ELROD
DRAWN BY RON KAUFMAN
SUPERVISED BY R.L.H. & H.B.
CHECKED BY K. ELROD

DATE 1-86
DATE 2-86
DATE 2-86
DATE 2-86

BRUNING 44-141 21318

[illegible]

LIST OF DRAWINGS	DWG. NO.
LAYOUT OF BRIDGE.....	M-379-15
GENERAL NOTES & ESTIMATED QUANTITIES.....	M-379-16
FOUNDATION DATA.....	M-379-17
FOUNDATION DATA.....	M-379-18
SUPERSTRUCTURE.....	M-379-19
SUPERSTRUCTURE DETAILS.....	M-379-20
PRESTRESSED I-BEAM DETAILS.....	M-379-21
ABUTMENT NO. 1.....	M-379-22
ABUTMENT NO. 1 DETAILS.....	M-379-23
ABUTMENT NO. 2.....	M-379-24
ABUTMENT NO. 2 DETAILS.....	M-379-25
BENT NO. 1.....	M-379-26
BENT NO. 1 DETAILS.....	M-379-27
BENT NO. 2.....	M-379-28
BENT NO. 2 DETAILS.....	M-379-29
FINAL FOUNDATION DATA.....	M-379-30
BILL OF STEEL.....	M-379-31
BILL OF STEEL.....	M-379-32

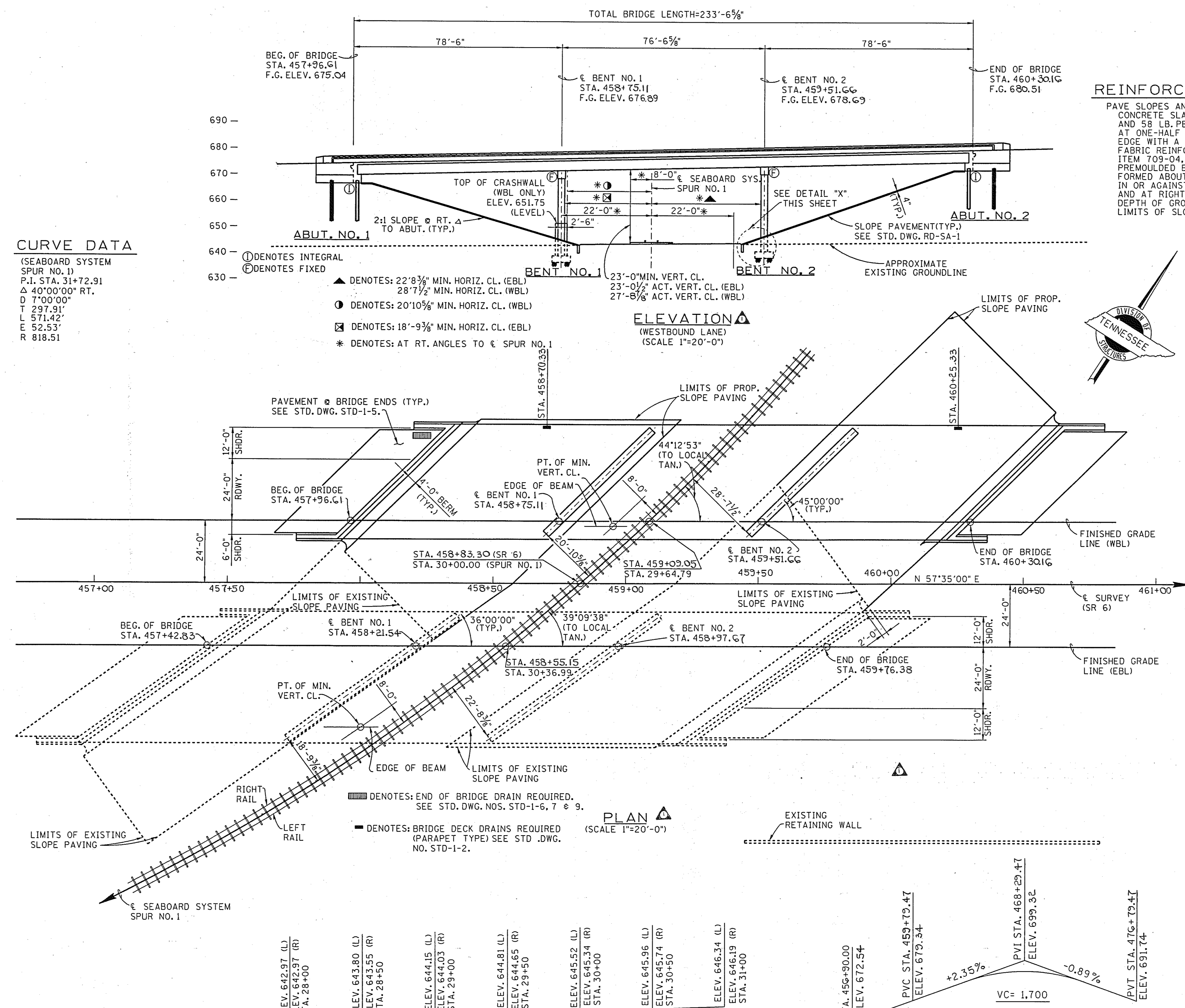
<u>LIST OF STANDARD DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
BRIDGE RAILING CONCRETE PARAPET.....	STD-1-1	4-28-97
STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS.....	STD-1-2	9-11-95
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS.....	STD-1-5	4-28-97
BRIDGE END DRAIN DETAILS 2'x8'-7" @ 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS.....	STD-1-6	4-28-97
BRIDGE END DRAIN DETAILS 2'x8'-7" @ 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS.....	STD-1-7	4-28-97
BRIDGE END DRAIN DETAILS 1'x8'-7" WITH PAVEMENT AT BRIDGE ENDS.....	STD-1-9	5-01-95
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS.....	STD-4-1	4-28-97
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA.....	STD-4-2	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS.....	STD-4-3	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS.....	STD-4-4	6-10-96
STD. PILE DETAILS.....	STD-5-1	10-25-93
STD. SEISMIC DETAILS.....	STD-6-1	6-10-96
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB.....	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT @ DRAINAGE DETAILS.....	STD-10-1	5-11-92
STD. DETAILS AND INTERMEDIATE DIAPHRAGM DETAILS FOR T-BEAMS.....	STD-14-2	
SLOPE PROTECTION.....	RD-SA-1	5-27-98

<u>LIST OF</u> <u>SPECIAL PROV.</u>	<u>PROV. NO.</u>	<u>LAST</u> <u>REV. DATE</u>
APPROVAL OF SHOP DRAWINGS.....	105A	12-15-97

2006 ADT = 16,130
42'-0" ROADWAY W/ STD-1-1 PARAPET (WBL)
DESIGN SPEED = 70 MPH
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 2
LAYOUT OF BRIDGE
STATE ROUTE 6
(WESTBOUND LANE)
OVER
SEABOARD SYSTEM
SPUR NO. 1
BRIDGE I.D. NO. 60SR0060037
▲ STATION 458+83.30
MAURY COUNTY
1999


CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

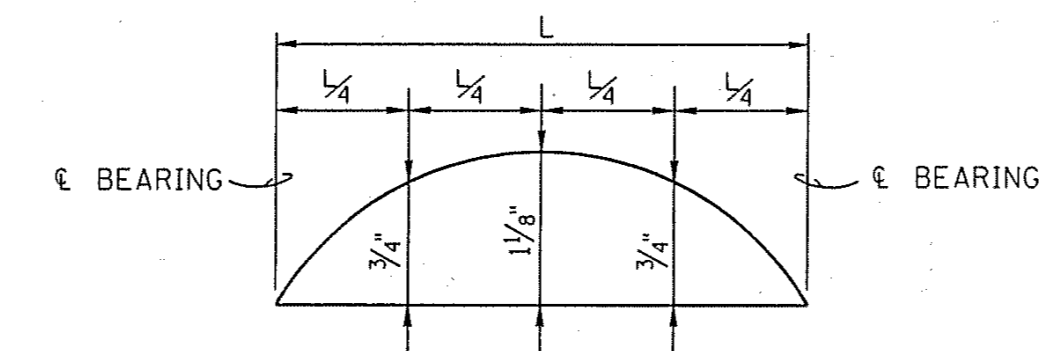
M-379-15



DESIGNED BY JIM SUMMERS DATE 02-99
 DRAWN BY FRED SHARPE DATE 03-99
 SUPERVISED BY J.W.F./C.M.D. DATE 03-99
 CHECKED BY JIM SUMMERS DATE 04-99

TOP OF RAIL ELEVATIONS

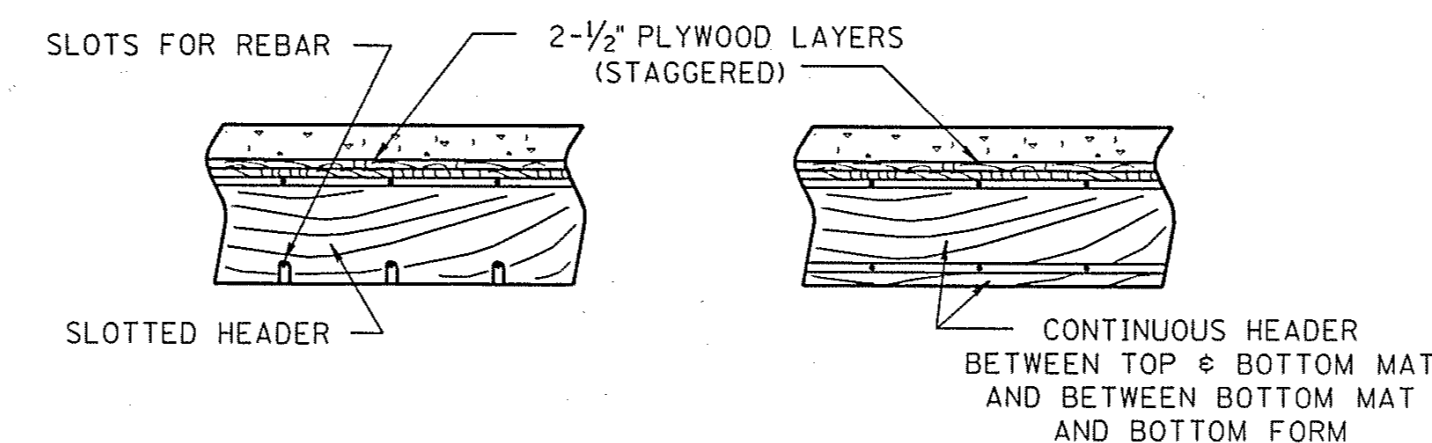
GRADE SKETCH 
STATE ROUTE 6
(ELEVATIONS BASED ON FINISHED GRADE)



DEAD LOAD CORRECTION CURVE; THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. WHEN POURING PARAPET, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS FOR PARAPET RAILS. ALSO SEE STD. DWG. NO. STD-1-1.

(LOOKING FORWARD ON SURVEY)



DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

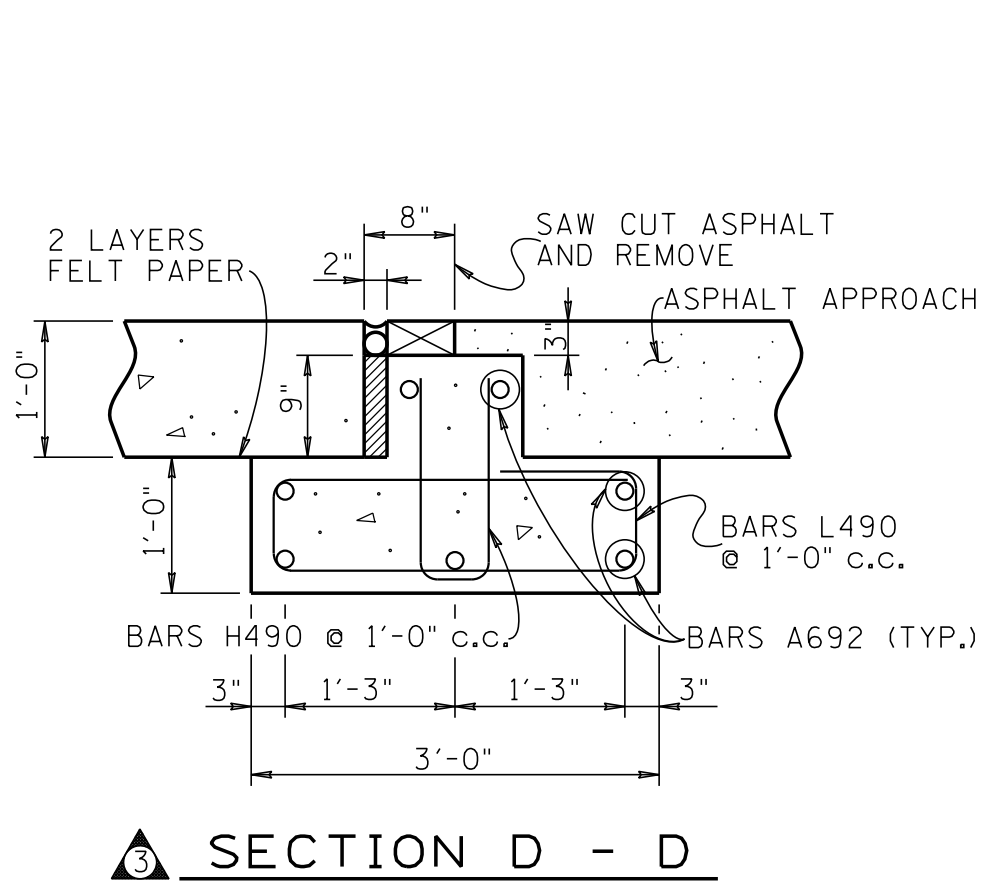
1. NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
2. THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.
3. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.

CLASS "D" CONCRETE (BRIDGE DECK) C.Y.	STEEL BAR REINFORCEMENT (BRIDGES) LB.	EPOXY-COATED REINFORCING STEEL (BRIDGES) LB.
290	2,145	87,089

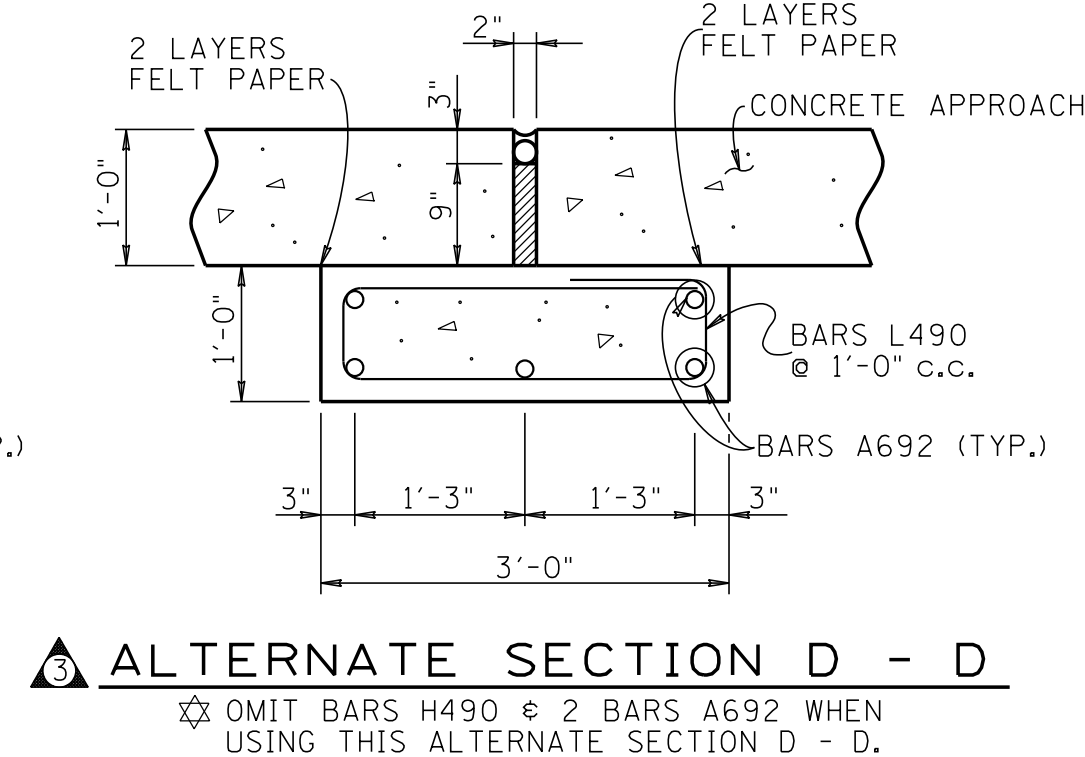
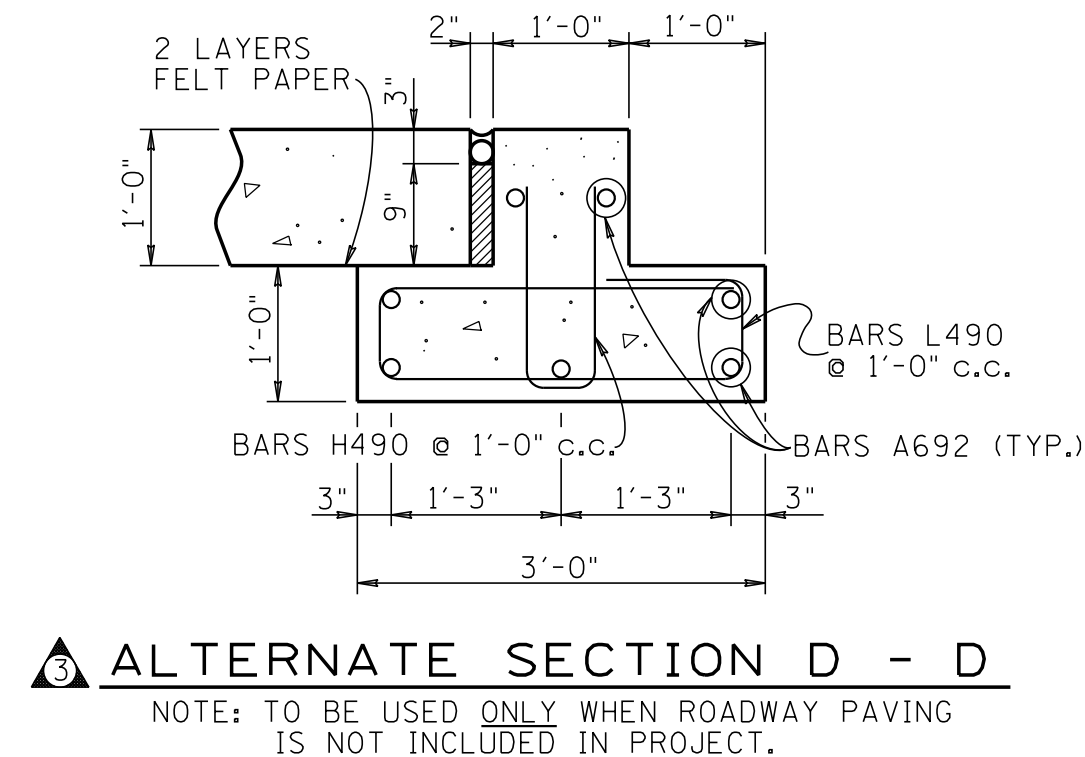
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 2
SUPERSTRUCTURE
STATE ROUTE 6
(WESTBOUND LANE)
OVER
SEABOARD SYSTEM SPUR NO. 1
▲ STATION 458+83.30
MAURY COUNTY
1999

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY JIM SUMMERS DATE 02-99
DRAWN BY G. W. KORNIKOSKI DATE 03-99
SUPERVISED BY C.A.D./J.W.F. DATE 03-99
CHECKED BY J. SUMMERS DATE 03-99



△ THESE NUMBERS VARY DEPENDING UPON ROADWAY WIDTH.

[illegible]

1. QUANTITIES FOR CLASS 'A' CONCRETE, REGULAR AND EPOXY COATED REINFORCING STEEL (WHEN REQUIRED FOR MEDIAN BARRIER), BACKER ROD, JOINT SEALER, ELASTOMERIC CONCRETE, STYROFOAM, GRATE AND MISCELLANEOUS MATERIALS FOR BRIDGE END DRAIN, WHEN REQUIRED, ARE TO BE INCLUDED IN PAVEMENT AT BRIDGE ENDS, S.Y. FOR BAR BENDING DIMENSIONS SEE THIS SHEET AND BILL OF STEEL FOR BRIDGE END DRAIN ON DRAWING NO. STD-1-6.
2. COST OF MINERAL AGGREGATE CLASS A GRADING D BASE QUANTITY SHALL BE INCLUDED IN COST OF PAVEMENT AT BRIDGE ENDS. CLASS B GRADING C OR D MAY ALSO BE USED.

CONCRETE: TO BE CLASS 'A' ($f'_c = 3,000$ psi)
REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED
OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE
TENNESSEE DEPARTMENT OF TRANSPORTATION (CURRENT EDITION).

NOTE: THE APPROACH SLAB SHALL NOT BE POURED UNTIL THE ADJACENT
END SPAN DECK SLAB IS IN PLACE AND ACCEPTED BY THE ENGINEER.

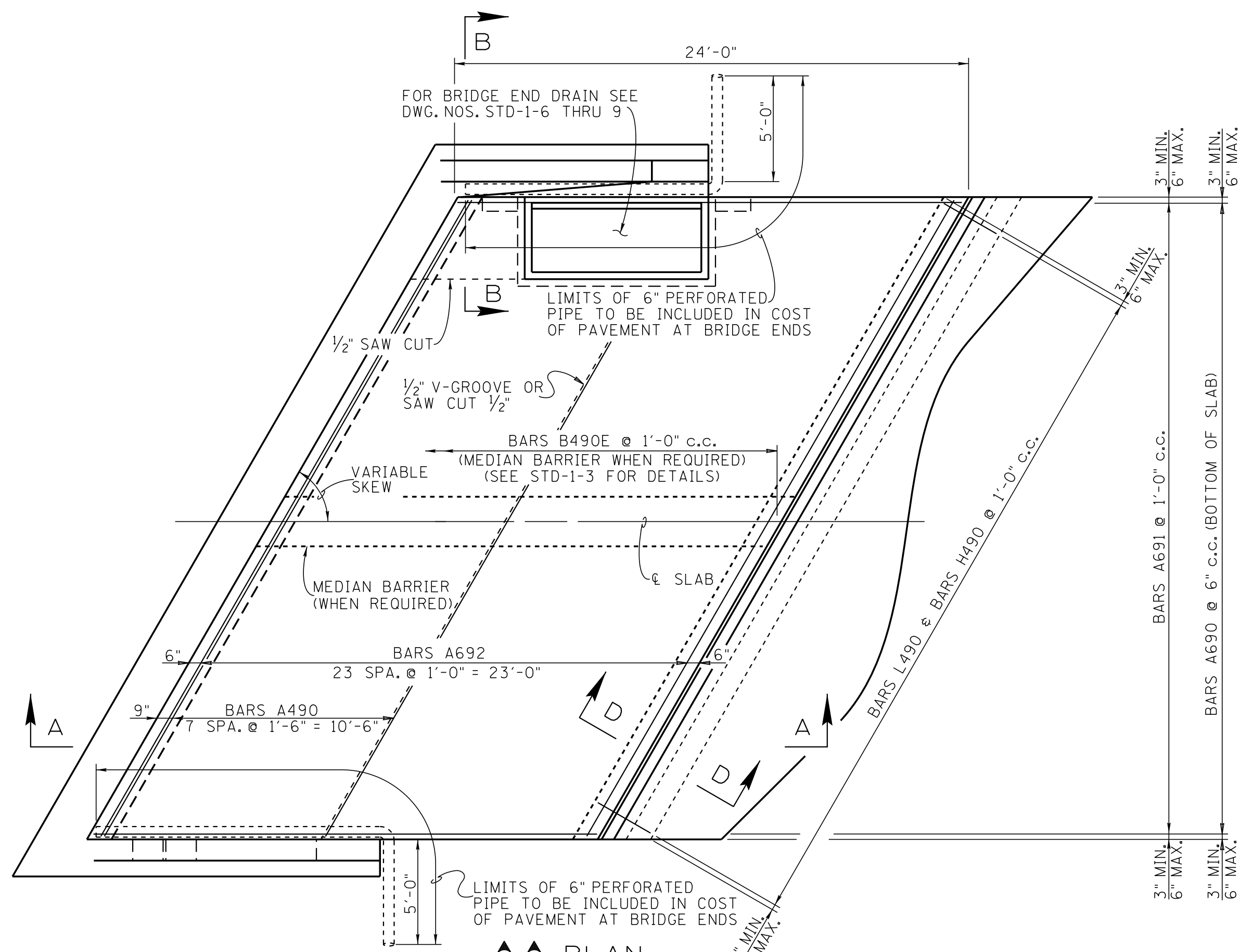
NOTE: THE APPROACH SLAB CONTROL ELEVATIONS SHALL BE ADJUSTED, (IF REQUIRED), SO AS TO MATCH THE IN PLACE DECK SLAB IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTIONS.

② ④ JOINT SEAL SYSSTEM;
 ③ THE EXPANSION JOINT SYSTEM USED SHALL BE ON THE TDOT
 ④ APPROVED QUALIFIED LISTS FOR ACCEPTABLE PRODUCTS.
 THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT
 SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED
 BY THE EXPANSION JOINT SUPPLIER.

M/R MINOR REVISION - FHWA
APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

REINFORCED CONCRETE
PAVEMENT AT BRIDGE ENDS
1995



PLAN
(90° SKEW)

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES